



7-ELEVEN

EXISTING STORE W/ FUELING REMODEL

7-ELEVEN STORE NO. 46895

SPEEDWAY STORE NO. 7645

1542 ROUTE 9

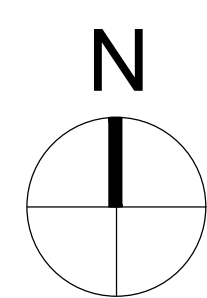
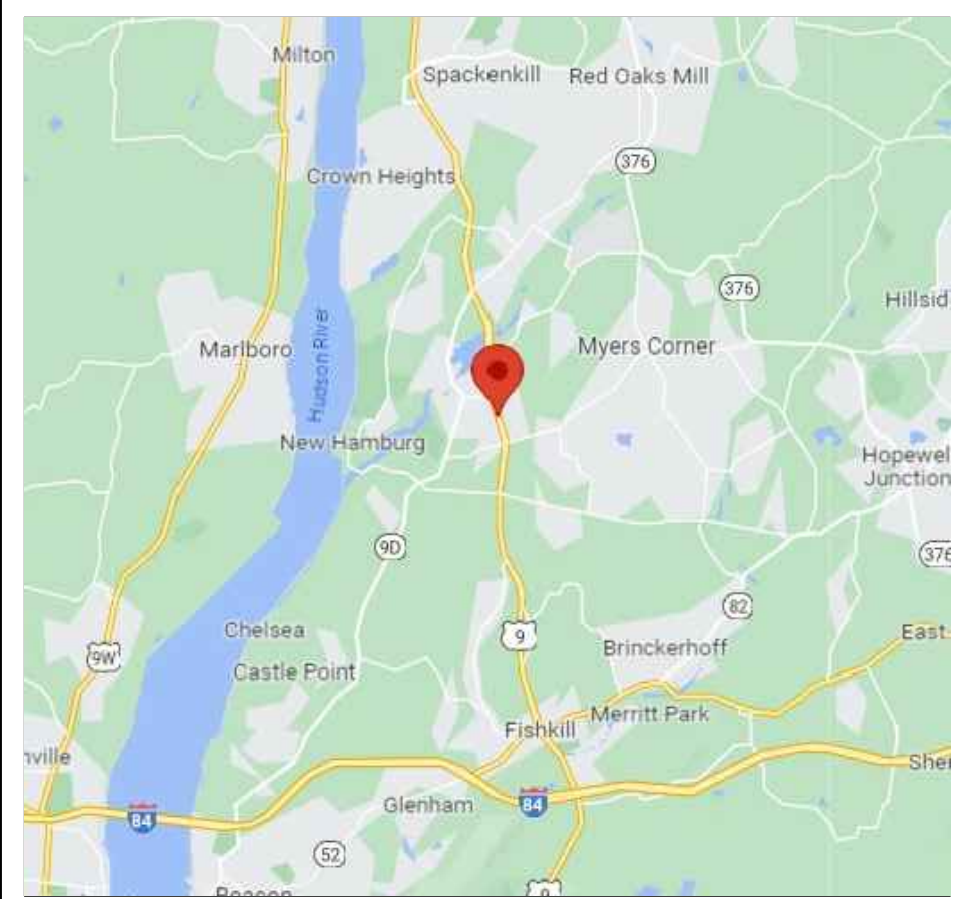
WAPPINGERS FALLS, NY 12590

SHEET INDEX

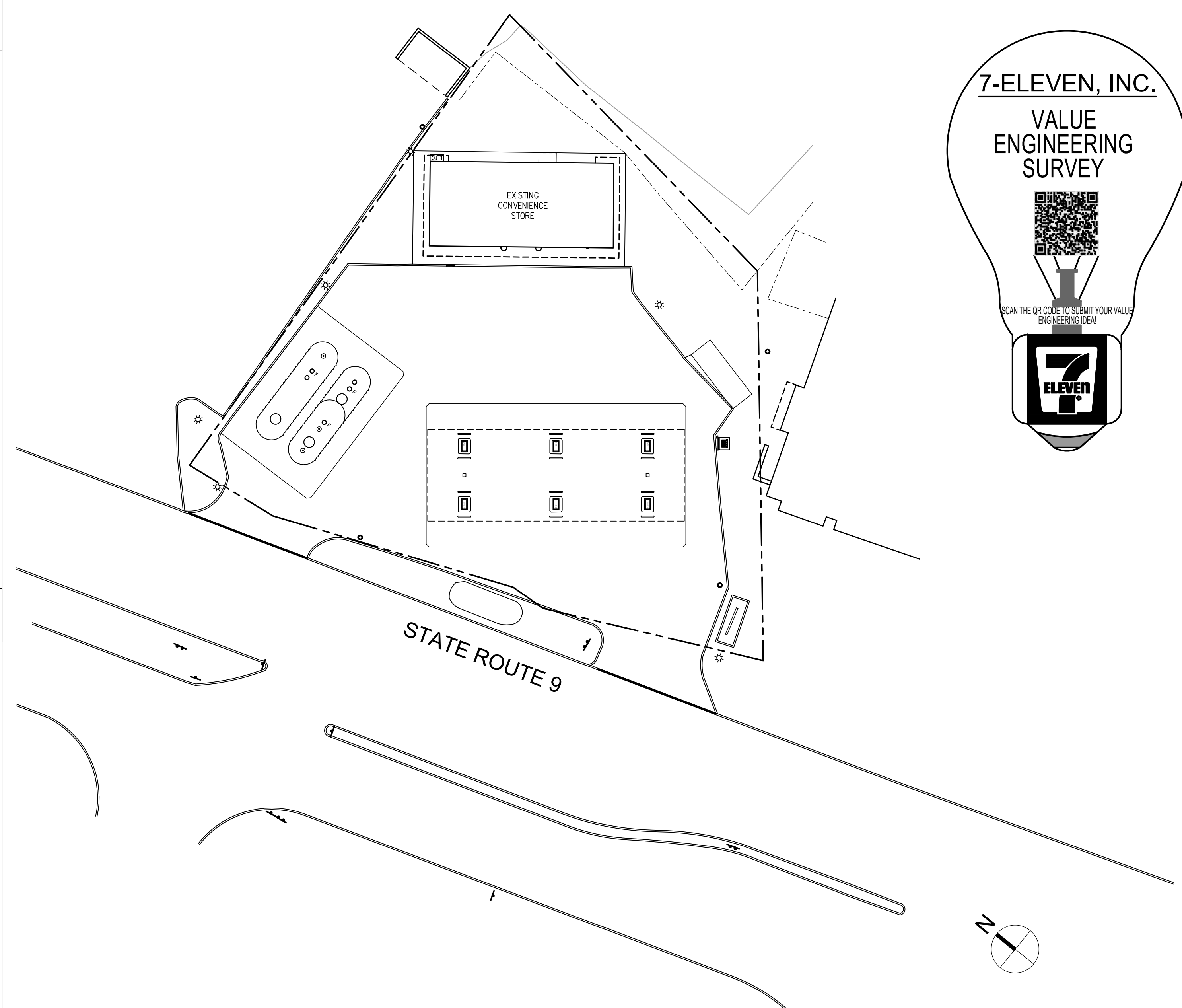
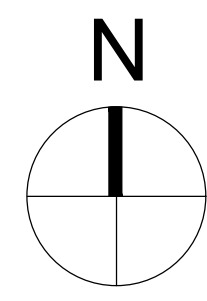
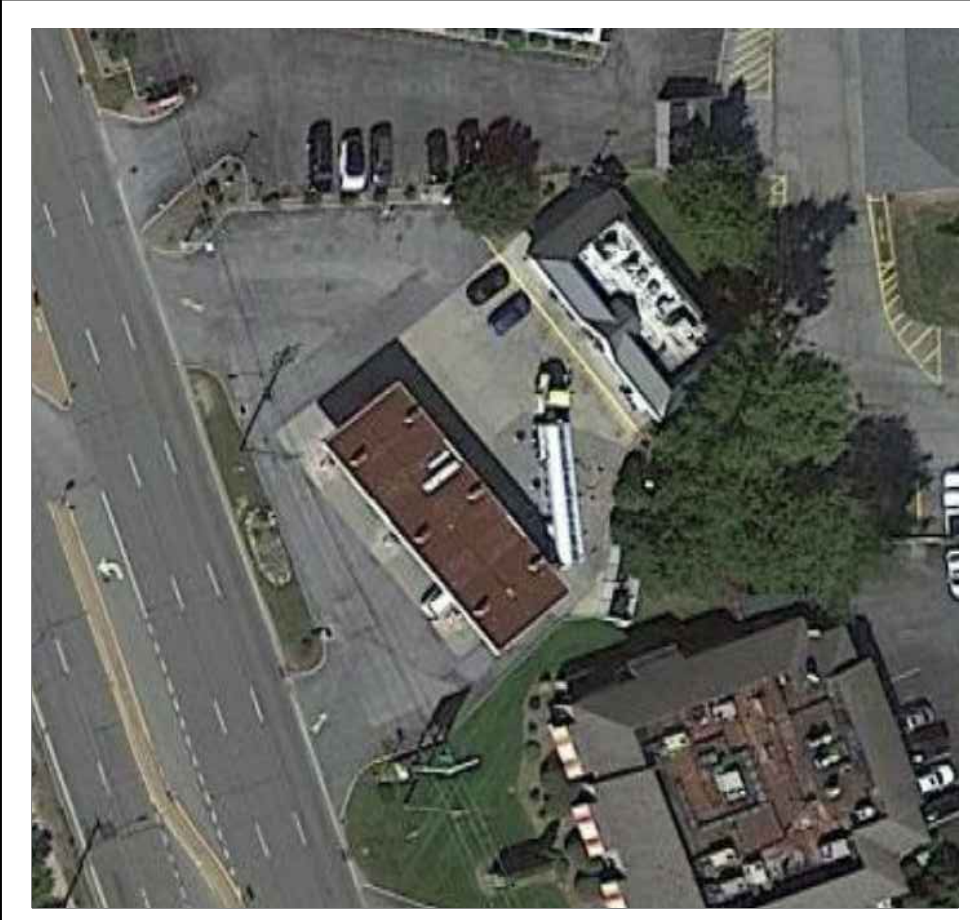
Rev. #	Date	Description
1	09/19/23	PEER REVIEW COMMENTS
		Proto 2022-04
		FUELING CANOPY AND UNDERGROUND STORAGE TANKS
G0.0		FUELING CANOPY COVER SHEET AND INDEX
G0.1		FUELING ARCHITECTURAL SITE PLAN
G0.2		FUELING TRUCK ROUTE
G0.3		FUELING DEMOLITION PLAN
G1.0		FUELING PIPING AND TANK PLAN
G2.0		NOT USED
G3.0		TANK SECTIONS
G4.0		TANK SUMP DETAILS - DUAL OUTPUT
G4.1		TANK SUMP DETAILS
G5.0		DISPENSER AND SUMP DETAILS
G6.0		REMOTE VENT AND DETAILS
G6.1		FUELING DIMENSIONAL PLAN
G7.0		EQUIPMENT SCHEDULES
G7.1		EQUIPMENT SCHEDULES
G8.0		FUELING CANOPY LAYOUT
G8.1		SIGNAGE DETAILS
G9.0		UST ELECTRICAL PLAN, ONE-LINE DIAGRAM AND PANEL SCHEDULE
G9.1		N.E.C. CLASSIFIED AREA
G10.0		MISCELLANEOUS ELECTRICAL DETAILS
G11.0		ISOLATION RELAY BOX DETAIL
G12.0		SITE MONITORING EQUIPMENT DIAGRAM
G13.0		FUELING SPECIFICATIONS
G14.0		FUELING SPECIFICATIONS
G15.0		FUELING SPECIFICATIONS
G16.0		FUELING SPECIFICATIONS
G17.0		FUELING SPECIFICATIONS
CPC-1		FUELING REMODEL PAVEMENT AND CURBING DETAILS
SCB-1		FUELING REMODEL STANDARD CANOPY BASE DETAILS

7-ELEVEN, INC.
 3200 HACKBERRY ROAD, IRVING TEXAS 75063
 7-ELEVEN #46895 - SPEEDWAY #7645
 1542 ROUTE 9
 WAPPINGERS FALLS, NY 12590
 FUELING CANOPY COVER SHEET
 AND INDEX

VICINITY MAP



SITE LOCATION MAP



PROJECT DATA

2020 BUILDING CODE OF NEW YORK STATE
 2020 MECHANICAL CODE OF NEW YORK STATE
 2020 PLUMBING CODE OF NEW YORK STATE
 2017 NATIONAL ELECTRICAL CODE
 2020 FIRE CODE OF NEW YORK STATE
 2020 FUEL GAS CODE OF NEW YORK STATE
 NFPA 30 FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE-2015 EDITION
 NFPA 30A CODE FOR MOTOR FUEL DISPENSING FACILITIES AND REPAIR GARAGES-2015 EDITION
 NFPA 1 UNIFORM FIRE CODE HANDBOOK

SUMMARY OF WORK

- GRADING AND SITEWORK
- ASSOCIATED UNDERGROUND TANKS AND PIPING.
- FUELING STATION DESCRIPTION:
- (4) NEW GILBARCO 3+0 DISPENSERS
- (2) NEW GILBARCO 3+1 DISPENSERS FOR DIESEL
- (1) 10" DIA. 20K GAL. DW FIBERGLASS UST FOR UNLEADED FUEL
- (1) 10" DIA. 20K GAL. SPLIT DW FIBERGLASS UST - 12K DIESEL/8K PREMIUM
- (2) 2.0 hp VARIABLE SPEED STP FOR RUL AND PUL
- (1) 1.5 hp FIXED SPEED STP FOR DIESEL
- (3) ATG's (1 PER STP SUMP)
- (3) PRODUCT FILL BUCKETS (1 EACH UST)
- 1.5" FLEX PIPING IN 4" CHASE PIPING FOR RUL, PUL AND DIESEL FUELS
- 2" SW FRP VENT PIPING (UST'S TO REMOTE VENT SUMP)
- (1) NEW BRAVO VENT BOX FOR RUL, PUL AND DIESEL
- (1) EXISTING FUELING CANOPY FOR SIX (6) DISPENSERS TO REMAIN
- (6) NEW 5' X 4' X 13" STAINLESS STEEL ISLANDS AT EACH DISPENSER
- NEW VEEDER ROOT TLS-450 PLUS SITE MONITOR CONSOLE
- (1) NEW HYDRX FUEL CONDITIONING SYSTEM AT DIESEL TANK

PROJECT DIRECTORY

OWNER: 7-ELEVEN INC
 3200 HACKBERRY ROAD
 IRVING, TX 75063
 MEP ENGINEER: CORE STATES GROUP
 212 SE 34TH STREET, SUITE 2
 BENTONVILLE, AR 72712
 CONTACT: DAVID LEIFFER
 479.986.4400
 EMAIL: DLEIFFER@CORE-STATES.COM

REFERENCE SYMBOLS

- NORTH ARROW
- HEIGHT ELEVATION INDICATOR
- DOOR TAG
- ENLARGED DETAIL INDICATOR
- GRID LINE INDICATOR
- WINDOW TAG
- WALL TAG
- xxxxxxx - ROOM TAG
- △ - REVISION DELTA
- - SPOT ELEVATION INDICATOR
- xx - KEY NOTE INDICATOR
- xx - FINISH INDICATOR
- ⊗ - SECTION CUT INDICATOR
- xx.x - ELEVATION INDICATOR
- xxx - EQUIPMENT TAG (FOR LIST REF G7.0)

CORE STATES GROUP
 ARCHITECTURE AND ENGINEERING P.C.
 CERTIFICATE OF AUTHORIZATION
 NO. 092855

Job#: SPW-36100-SEI
 Scale: AS NOTED
 Date: 02/14/23
 Drawn By: RMC
 Checked By: CGC

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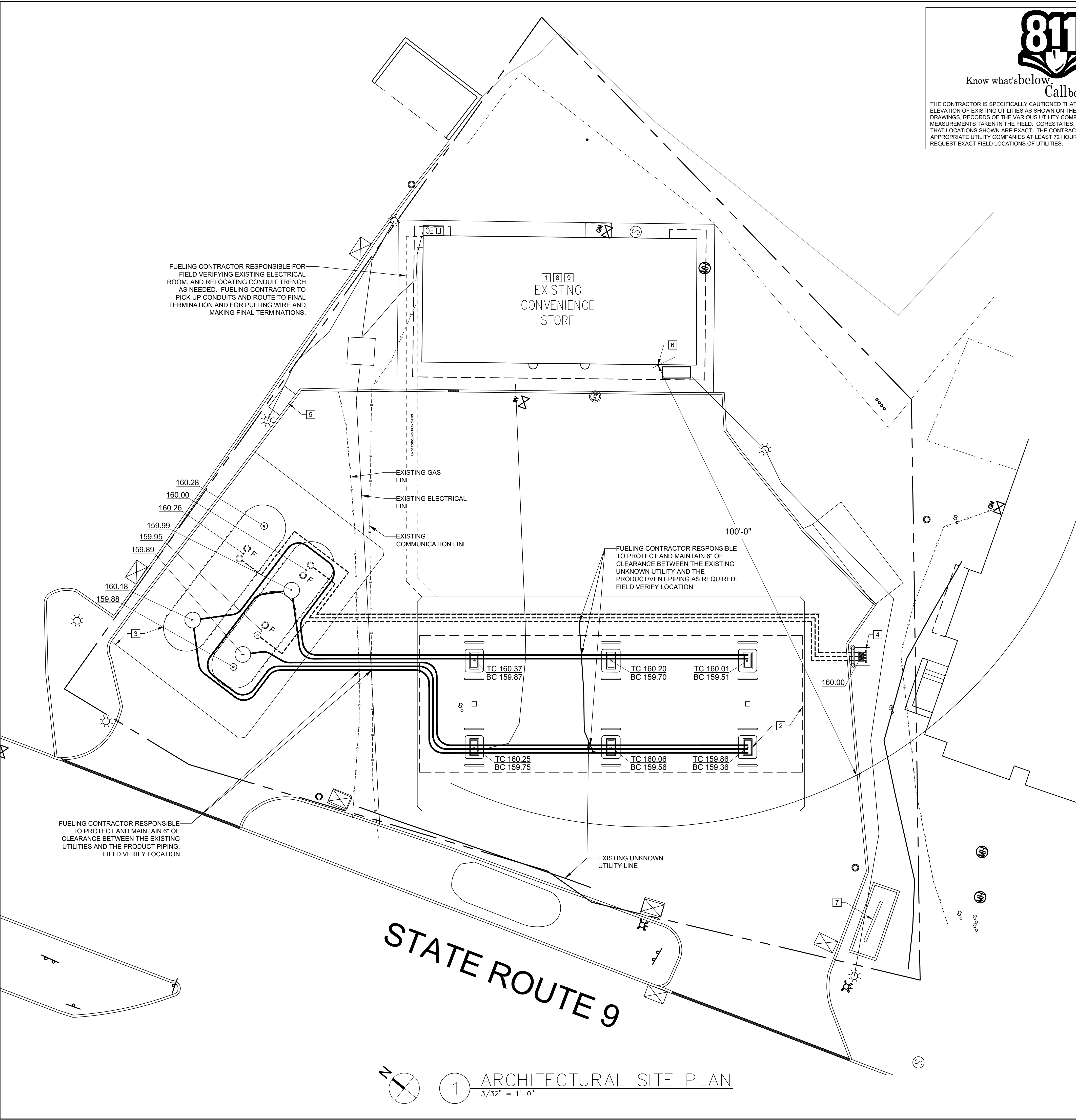


SHEET: **G0.0**
 FUELING - USA

PRINTED BY: CCARR
 DRAWING NAME: 46895 - G0.1 FUELING ARCHITECTURAL SITE PLANING
 PRINT DATE: Sep 19, 2023 - 10:05am

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON DESIGN DRAWINGS, RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. CORE STATES, INC. DOES NOT GUARANTEE THAT LOCATIONS SHOWN ARE EXACT. THE CONTRACTOR MUST CONTACT THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF UTILITIES.

GENERAL NOTES	SCOPE OF WORK
<p>FUEL CONTRACTOR</p> <p>1 FUEL GC TO WORK WITH SEI PM ON SCHEDULING. ANY CONFLICTS SHALL BE BROUGHT TO THE ATTENTION OF THE SEI PROJECT MANAGER.</p> <p>2 FUEL GC RESPONSIBLE FOR ALL CONCRETE OVER TANKS AND PRODUCT PIPING (I.E. TANK SLAB AND DRIVE MATS). CONCRETE CHAIRS ARE BY SEI.</p> <p>3 FUEL GC RESPONSIBLE FOR CANOPY FOOTINGS.</p> <p>4 FUEL GC TO INSTALL CANOPY DRAINS (WHERE APPLICABLE) TO POINT OF CONNECTION WITHIN (10) FT OF DRIVE MATS.</p> <p>5 FUEL GC TO INSTALL ALL FUEL RELATED CONDUITS, SEAL-OFFS, ELECTRICAL J-BOXES PER STATE, LOCAL, MANUFACTURERS AND SEI CODES AND SPECIFICATIONS.</p> <p>6 FUEL GC RESPONSIBLE FOR ALL ELECTRICAL CONDUITS, SEAL-OFFS, ELECTRICAL J-BOXES PER STATE, LOCAL, MANUFACTURERS AND SEI CODES AND SPECIFICATIONS.</p> <p>7 FUEL GC TO PULL WIRE FOR CANOPY LIGHTING, INTERCOM, DISPENSERS, TURBINES AND MONITORING SYSTEM DEVICES.</p> <p>8 FUEL GC TO MAKE ALL FINAL CONNECTIONS TO FUELING EQUIPMENT AND ELECTRONICS.</p> <p>9 FUEL GC TO PULL TRADE PERMITS FOR FUELING WORK (AS REQUIRED).</p> <p>10 FUEL GC TO "CALL FOR" AND "BE PRESENT FOR" ALL LOCAL REGULATOR INSPECTIONS AND RESPONSIBLE FOR SCHEDULING ON-SITE INSPECTIONS WITH APPROPRIATE SEI REPRESENTATIVE RELATED TO FUELING.</p> <p>11 FUEL GC TO COORDINATE WITH TANKNOLOGY AND THE SEI CONSTRUCTION MANAGER FOR TESTING OF FUELING SYSTEM.</p> <p>12 FUEL GC TO HAVE PERSONNEL ON SITE FOR TESTING.</p> <p>13 FUEL GC RESPONSIBLE FOR CLOSE OUT PACKAGE PER 7-11 MATRIX.</p>	<p>1 INSTALL NEW XERXES 20,000 GALLON 10' DIA DOUBLE WALL FIBERGLASS UNDERGROUND STORAGE TANK (UST) FOR REGULAR FUEL.</p> <p>2 INSTALL NEW XERXES 20,000 GALLON 10' DIA DOUBLE WALL FIBERGLASS UNDERGROUND SPLIT STORAGE TANKS (UST'S) FOR 12,000 GALLONS DIESEL AND 8,000 GALLONS PREMIUM FUELS.</p> <p>3 INSTALL (1) NEW FE PETRO 1.5 HP FIXED SPEED SUBMERSIBLE PUMP WITH INTAKE FILTER FOR DSL FUEL.</p> <p>4 INSTALL (2) NEW FE PETRO 2 HP VARIABLE SPEED SUBMERSIBLE PUMP WITH INTAKE FILTER FOR RUL AND PUL FUELS.</p> <p>5 INSTALL (4) NEW GILBARCO 700S 3+0 DISPENSERS AND (2) NEW GILBARCO 700S 3+1 DISPENSERS.</p> <p>6 INSTALL NEW OPW LOOP SYSTEM FROM NEW UST'S TO NEW DISPENSERS.</p> <p>7 INSTALL (3) NEW VAPOR/VENT RISERS WITH BRAVO VENT BOX AND BRAVO VAPOR VENT RACK FOR SUPPORT.</p> <p>8 INSTALL NEW 2" FIBERGLASS PIPING FROM VAPOR/VENT AT UST'S TO NEW REMOTE VENT.</p> <p>9 INSTALL NEW CONCRETE AT DISTURBED AREAS AT TANK SLAB FOR NEW PIPING PER SEI STANDARDS.</p> <p>10 INSTALL NEW CONCRETE AT DISTURBED AREAS AT CANOPY SLAB FOR NEW PIPING PER SEI STANDARDS.</p> <p>11 INSTALL NEW "U" BOLLARDS AT DISPENSERS AND REMOTE VENT TO SEI SPECIFICATIONS (TYP 13).</p> <p>12 ACTIVATE INTERCOMS INSIDE OF THE DISPENSERS.</p> <p>13 INSTALL (2) NEW FIRE EXTINGUISHERS AT ADA COMPLIANT HEIGHT AT CANOPY COLUMNS.</p> <p>14 GC TO PROVIDE AND INSTALL (6) NEW TRASH CANWASH BUCKET COMBOS AT CANOPY COLUMNS.</p> <p>15 EXISTING MAIN ID SIGN TO REMAIN. RE-IMAGE AS REQUIRED.</p> <p>16 PAINT LED SIGNAGE FRAME, POLES, NEW BOLLARDS, CANOPY COLUMNS PER SPEEDWAY SPECIFICATIONS.</p> <p>17 INSTALL NEW VEEDER ROOT TLS-450 PLUS SITE MONITOR CONSOLE. FIELD VERIFY LOCATION.</p> <p>18 ROUTE NEW CONDUIT TO NEW ELECTRICAL THROUGH EXTERIOR OF BUILDING, VERIFY LOCATION WITH SEI CONSTRUCTION MANAGER.</p> <p>19 DEMOLITION-REMOVE EXISTING TANKS, TANK SLAB, EXISTING UST SYSTEM AND PIPING COMPLETELY. REMOVE ADDITIONAL PAVEMENT/SIDEWALKS AS REQUIRED TO FACILITATE OTHER REMOVALS AND INSTALLATIONS.</p> <p>20 INSTALL (1) NEW E-STOP ON BUILDING EXTERIOR AT ADA COMPLIANT HEIGHT.</p> <p>21 EXISTING SPEEDWAY BRANDED CANOPY TO REMAIN FOR (6) NEW DISPENSERS.</p> <p>22 INSTALL (1) NEW INTERIOR E-STOP BEHIND THE SALES COUNTER.</p> <p>23 INSTALL (6) NEW 5' X 4' X 13" STAINLESS STEEL ISLANDS AT EACH DISPENSER.</p> <p>24 REUSE EXISTING POS. UPGRADE AS REQUIRED.</p> <p>25 REUSE EXISTING LED PUMP TOPPERS AT NEW DISPENSERS.</p> <p>26 INSTALL (1) NEW HYDRX FUEL CONDITIONING SYSTEM IN THE DIESEL STP SUMP.</p>
<p>KEY NOTES</p> <p>1 EXISTING CONVENIENCE STORE</p> <p>2 EXISTING FUELING CANOPY AND NEW DISPENSERS</p> <p>3 NEW UNDERGROUND FUEL STORAGE TANKS AND SLAB</p> <p>4 NEW FUEL STORAGE TANK VENTS</p> <p>5 EXISTING AIR MACHINE</p> <p>6 NEW E-STOP ON BUILDING EXTERIOR</p> <p>7 EXISTING MAIN ID SIGN TO REMAIN</p> <p>8 NEW VEEDER ROOT TLS-450 PLUS SITE MONITOR CONSOLE. FIELD VERIFY LOCATION</p> <p>9 NEW INTERIOR E-STOP BEHIND SALES COUNTER. FIELD VERIFY LOCATION</p>	



NOTE:
 TANKS TO BE BURIED AT A MINIMUM OF 4.17' AT THE LOWEST FINISH GRADE ELEVATION AND A MAXIMUM OF 5.67' AT THE HIGHEST FINISH GRADE ELEVATION ON THE TANK PAD. VENT PIPING MUST HAVE A MINIMUM SLOPE OF 1/8" PER FOOT FROM VENT SUMP TO THE TANKS. GC TO COORDINATE ANY BURIAL DEPTH DEVIATIONS WITH SEI PROJECT MANAGER.

NOTE:
 THIS DRAWING IS TO BE USED AS A SCHEMATIC DRAWING. IT IS THE RESPONSIBILITY OF THE GC AND FUELING SUB-CONTRACTORS INSTALLING PRODUCT PIPING AND FUEL SYSTEM TO VERIFY EXISTING FIELD CONDITIONS. IF FIELD CONDITIONS VARY FROM DRAWING AS SHOWN OR INHIBIT INSTALLATION AS SHOWN CEASE WORK AND CONTACT SEI PROJECT MANAGER OR REPRESENTATIVE.

GC AND SUB-CONTRACTORS TO FOLLOW REMOVAL, AND INSTALLATION REQUIREMENTS OF FUELING SYSTEM COMPONENTS PER MANUFACTURER INSTRUCTIONS, LOCAL, FEDERAL, AND FIRE MARSHALL'S MANDATES, LAWS, AND REGULATIONS.

COMPONENT	REGULAR	PREMIUM	VENT
A. FINISH GRADE ELEV. AT BEGINNING OF RUN (FT):	159.87	159.75	160.00
B. PIPE BURY (FT):	1.50	1.50	2.00
C. ELEVATION OF PIPE AT BEGINNING OF RUN (A-B)	158.37	158.25	158.00
D. PIPE LENGTH (LONGEST RUN) (FT):	97.00	105.00	171.00
E. PIPING RUN FALL (FT) (D*1/8" PER FT):	1.01	1.09	1.78
F. TOTAL CROSSOVERS/SWING JOINTS:	0.00	0.00	1.00
G. CROSSOVER/SWING JOINT DEPTH (FT):	0.00	0.00	0.75
H. ELEVATION AT END OF PIPE (FT) (C-E-G*F):	157.36	157.16	155.47
I. ADD FOR EXTRACTOR TEE AT SUMP:	1.25	1.25	0.50
J. TOP TANK ELEVATION (FT) (H-I):	156.11	155.91	154.97
K. HIGHEST FINISH GRADE ELEVATION AT TANK SLAB (FT):	160.28	160.28	160.28
L. MAX BURY DEPTH (K-J) (FT): MAX = 5.67'	4.17	4.37	5.31
M. LOWEST FINISH GRADE ELEVATION AT TANKS (FT):	159.88	159.88	159.88
N. MIN BURY DEPTH (M-J) (FT): MIN = 4.17'	3.77	3.97	4.91

TANK BURIAL DEPTH IS GOVERNED BY THE REGULAR VENT PIPING. THE TOP OF TANKS SHALL BE SET AT 154.97 FT.

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 7-ELEVEN #46895 - SPEEDWAY #7645
 1542 ROUTE 9
 WAPPINGERS FALLS, NY 12590
 FUELING ARCHITECTURAL SITE PLAN

Job#: SPW-36100-SEI
 Scale: AS NOTED
 Date: 02/14/23
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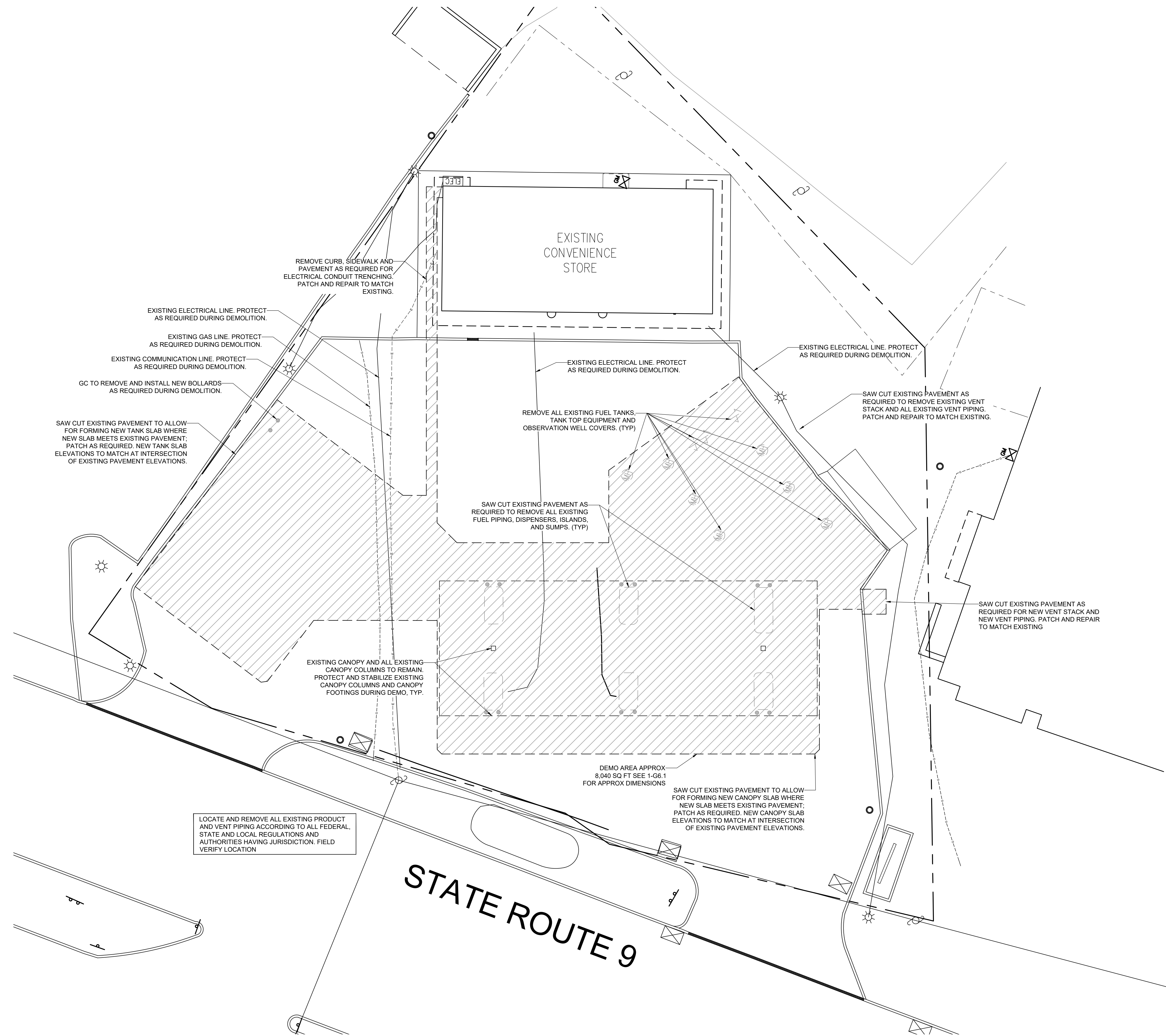
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STATE OF NEW YORK
 OFFICE OF THE STATE ENGINEER
 092855
 09-19-2023

SHEET:
G0.1
 FUELING - USA

GENERAL NOTES

- CONTRACTOR TO CUT AND REMOVE CONCRETE AS NECESSARY TO REMOVE ALL EXISTING PIPING. LOCATION OF REMOVAL AT DISPENSER ISLANDS IS APPROXIMATE AND IS BASED ON AVAILABLE INFORMATION.



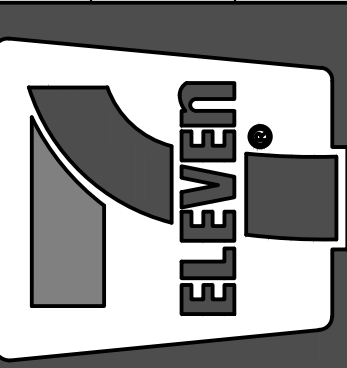
Rev. #	Date	Description

Proto 2022-04

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063

7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590

FUELING DEMOLITION PLAN



CORE STATES GROUP

ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 021079

46 East Main Street
Suite 204
Sayreville, NJ 08876
908.462.9700
core-states.com

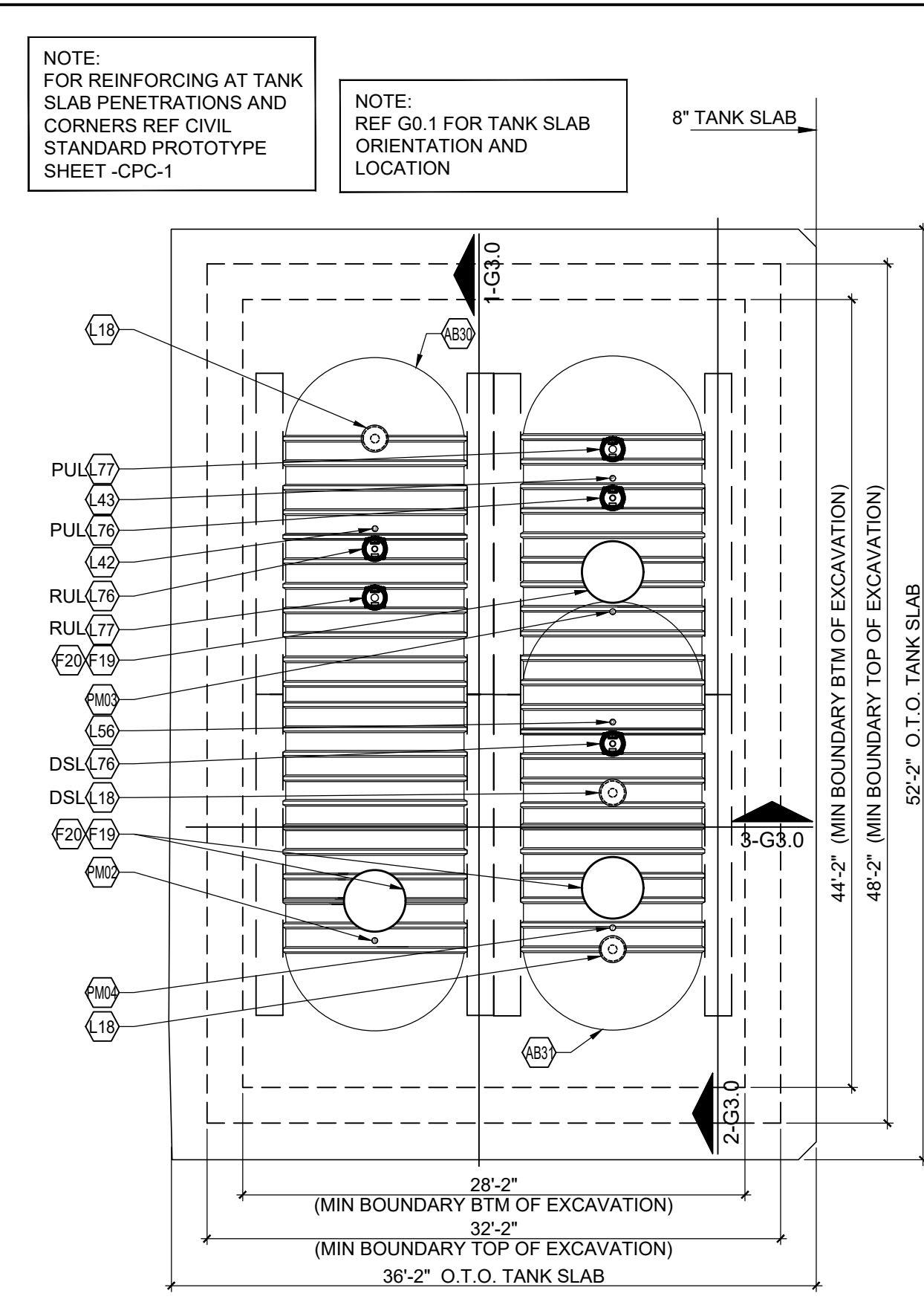
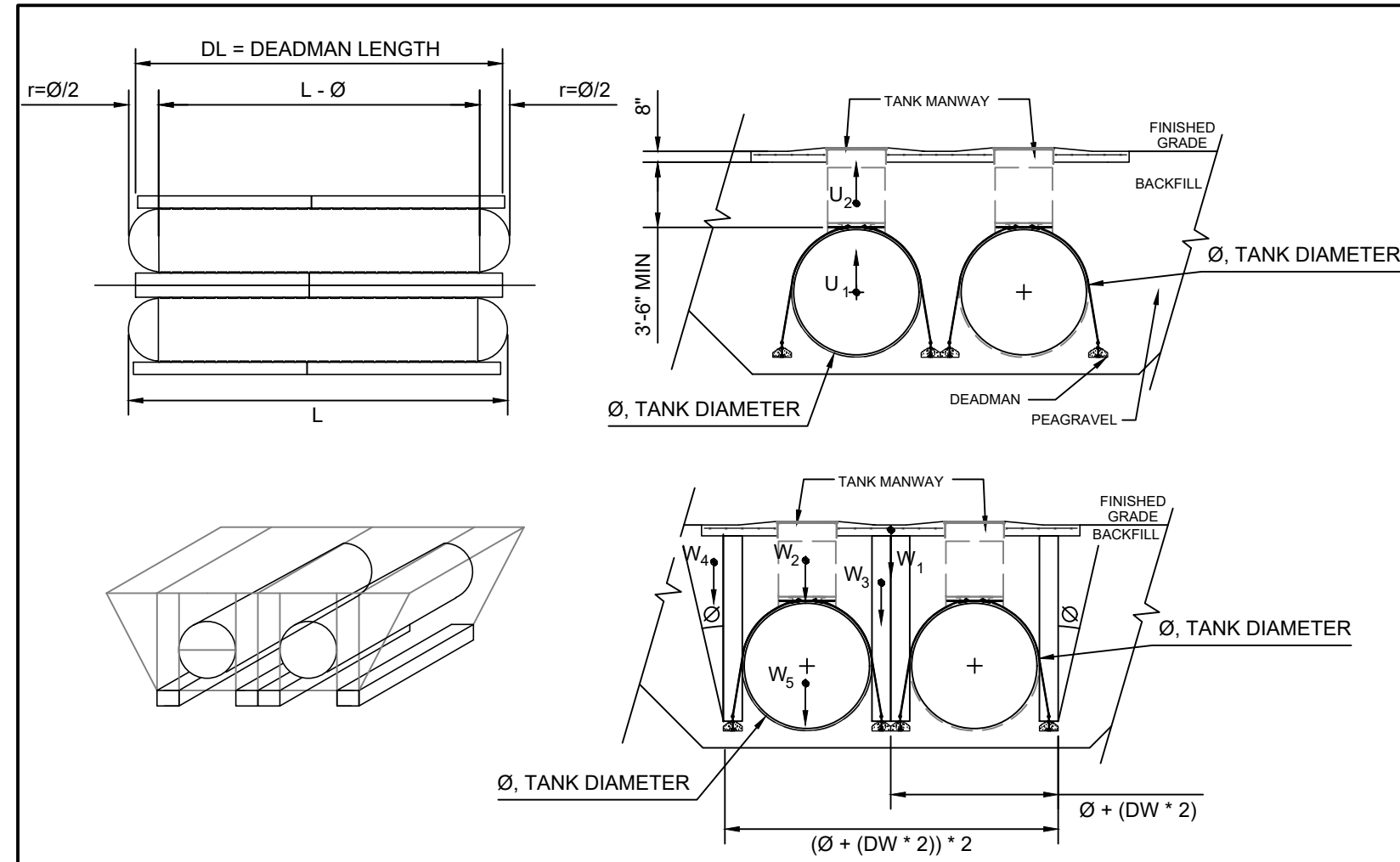
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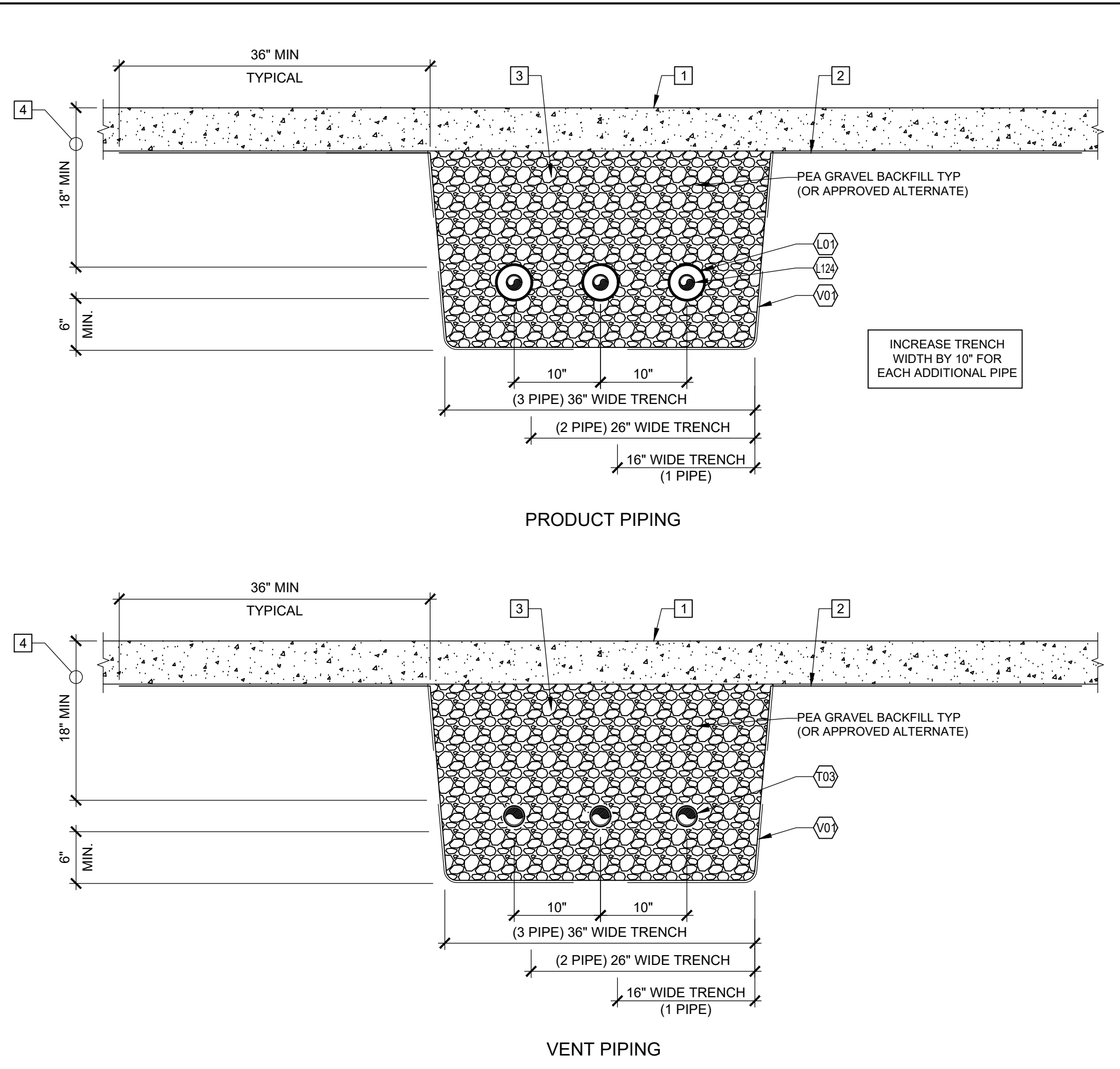


SHEET:
G0.3
FUELING - USA

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2 TANK SLAB PLAN
1/8"=1'-0"



3 PIPING TRENCH SECTION
1"=1'-0"

- ### GENERAL NOTES
- ALL CONSTRUCTION SHALL MEET COUNTY STANDARDS. CONTRACTOR TO SECURE ENTIRE CONSTRUCTION AREA WITH CHAIN LINK FENCING DURING CONSTRUCTION PERIOD.
 - THE CONTRACTOR SHALL IMMEDIATELY NOTIFY ENGINEER OF ANY DIFFERENCES BETWEEN THE ACTUAL FIELD CONDITIONS AND THE PLANS BEFORE PROCEEDING WITH CONSTRUCTION.
 - FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR THE INTEGRITY OF THE PIPING SYSTEM DURING CANOPY ERECTION AND CONCRETE FLATWORK INSTALLATION.
 - AFTER COMPLETING INSTALLATION OF DISPENSERS, THE FUEL CONTRACTOR IS TO INSTALL BRIGHT ORANGE CONSTRUCTION FENCE AROUND ENTIRE DISPENSER.
 - FUEL CONTRACTOR IS TO BE AWARE THAT ON THE DAY THE GASOLINE SYSTEM IS PUT INTO SERVICE, THE FUEL CONTRACTOR AND A GILBARCO AUTHORIZED SERVICE CONTRACTOR MUST BE ON SITE ONE (1) HOUR PRIOR TO STARTUP AND MUST REMAIN ON SITE THREE (3) ADDITIONAL HOURS FOR A TOTAL OF FOUR (4) HOURS. ANY COSTS INCURRED OVER FOUR (4) HOURS DUE TO FUELING SYSTEM ISSUES, SHOULD BE BILLED AS A CHANGE ORDER.

- ### KEY NOTES
- 6" CONCRETE SLAB AT FUELING CANOPY. REF G0.1 FOR TOS ELEVATIONS.
 - GEOTECH FABRIC MATERIAL TO EXTEND 36" PAST EACH SIDE OF PIPING TRENCH. ALL GEOTECH FABRIC MATERIAL SHALL BE PINNED PRIOR TO BACKFILLING. METHOD OF PINNING FABRIC AT TOP OF EXCAVATION TO BE APPROVED BY THE 7-ELEVEN CONSTRUCTION MANAGER.
 - PIPING TRENCHES SHALL BE DUG IN SUCH A MANNER THAT THE TRENCH WIDTH IS EQUAL TO AT LEAST TWICE WIDTH OF ALL PIPING CONTAINED WITHIN. MAINTAIN A MINIMUM OF 4" (10cm) CLEAR DISTANCE BETWEEN PIPING POSITIONED IN THE TRENCH AND 6" WHERE PIPING CROSSES OVER EACH OTHER. THE BOTTOM OF THE TRENCH SHALL BE AS UNIFORM AS POSSIBLE TO ELIMINATE HIGH SPOTS AND ENSURING AN EVEN LAYER OF BEDDING MATERIAL UNDER THE PIPE. REMOVE ALL SHARP ROCKS AND DEBRIS FROM THE TRENCH BOTTOM BEFORE BEDDING MATERIAL IS INSTALLED.
 - SITE CONDITIONS MAY CAUSE VARIATIONS WHICH MUST BE APPROVED BY 7-ELEVEN AND MANUFACTURER. PIPING BURIAL DEPTHS LESS THAN 18" REQUIRES 7-ELEVEN APPROVAL.

Underground Storage Tank Buoyancy Calculations for Fiberglass Tanks

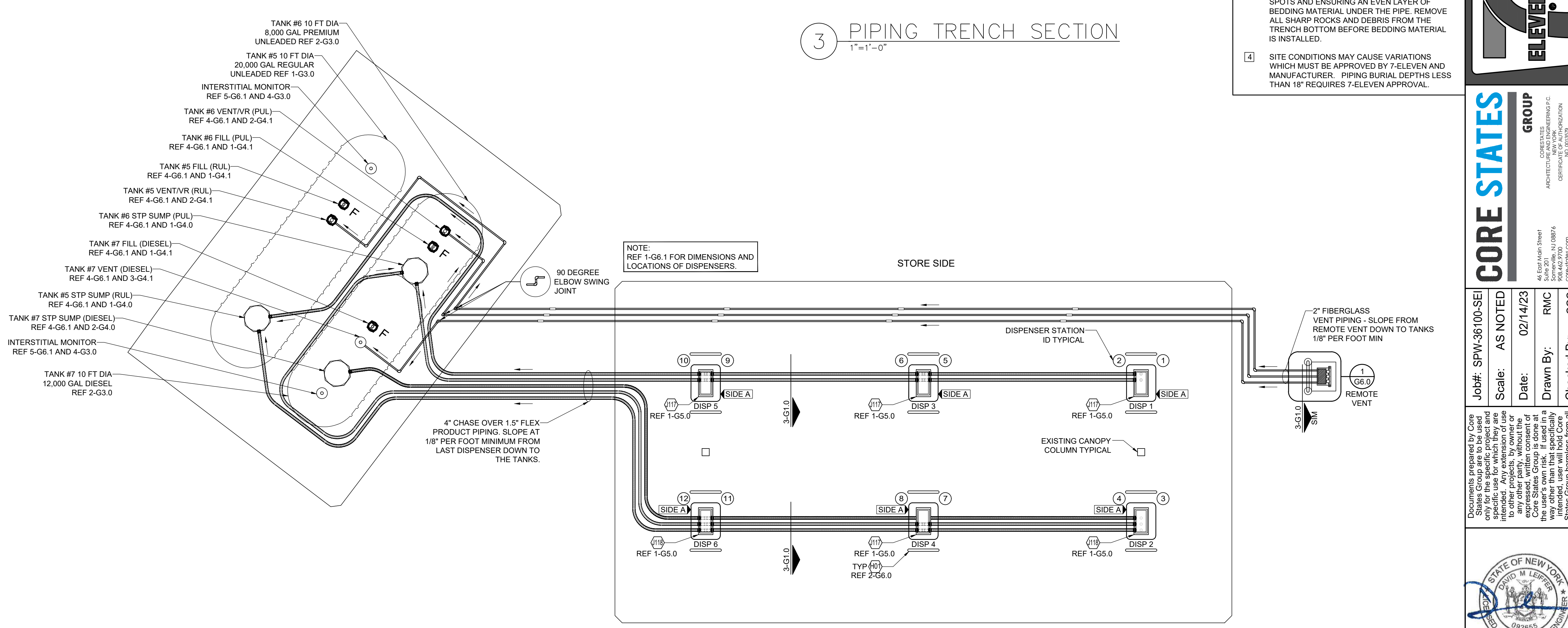
Project #:	SPW 36100 SE1
Project Name:	Wappingers Falls, NY #46895
Tank Manufacturer / Model:	Xerxes DW 10' 20K

Number of Tanks:	2	Assumptions
Tank Dry Weight:	9000 lbs	100% saturation to finished grade
Tank Volume:	20000 gallons	water = 62.4 lb/CF
Tank Diameter (#):	10 feet	submerged concrete = 87.6 lb/CF
Tank Bury Depth:	4.91 feet	submerged aggregate = 57.6 lb/CF
Tank Length (L):	37.75 feet	
Number of Sumps:	3	
Sump Diameter:	42 inches	
Tank Slab Length:	52.17 feet	
Tank Slab Width:	36.12 feet	
Tank Slab Thickness:	8 inches	
Deadman Length (DL):	30 feet	
Deadman Width (DW):	18 inches	
Deadman Height:	8.75 inches	

Tank & Sump(s) Uplift	U1 =	U2 =	U = Σ U1 =
Tank	$\frac{40000 \text{ gal}}{7.49 \text{ gal/CF}} \times 62.4 \text{ lb/CF} = 333,244 \text{ lbs}$	$141.6 \text{ CF} \times 62.4 \text{ lb/CF} = 8,839 \text{ lbs}$	342,083 lbs UP
Sumps			
Total Uplift			

Dead Weights for Hold Down	W1 =	W2 =	W3 =	W4 =	W5 =	W = Σ W1 =
Concrete Slab	$1258.0 \text{ CF} \times 87.6 \text{ lb/CF} = 110,200 \text{ lbs}$	$3,911 \text{ CF} \times 57.6 \text{ lb/CF} = 225,275 \text{ lbs}$	$3076.56 \text{ CF} \times 57.6 \text{ lb/CF} = 177,210 \text{ lbs}$	$332.4535 \text{ CF} \times 57.6 \text{ lb/CF} = 19,149 \text{ lbs}$	$157.5 \text{ CF} \times 87.6 \text{ lb/CF} = 13,797 \text{ lbs}$	563,631 lbs DOWN
Backfill Above Tank						
Gravel Column Above Deadman (2 deadman per tank)						
Soil Wedge Above Deadman (2 total)						
Concrete Deadman (2 deadman per tank)						
Dry Weight of Tank					18,000 lbs	
Total Weight						

Net Force	Fnet =	Σ W1 =	Σ U1 =	Fnet =
		563,631 lbs	342,083 lbs	221,548 DOWN
Factor of Safety	F.S. =			1.65 PASS



1 FUEL PIPING PLAN
1/8"=1'-0"

Job#:	SPW-36100-SE1
Scale:	AS NOTED
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Checked By:	CGC

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CORE STATES GROUP
CONCRETES
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 092855
46 East Main Street
Suite 200A, NJ 08076
908.462.9700
core-states.com

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SEAL OF THE STATE OF NEW YORK
OFFICE OF THE ENGINEER
092855
09-19-2023

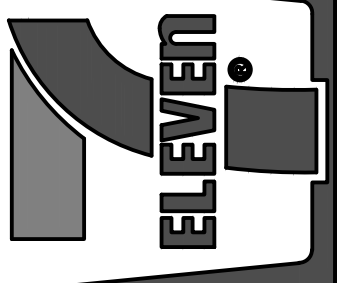
SHEET:
G1.0
FUELING - USA

KEY NOTES

- 1 ELECTRICAL CONDUIT(S) TO ENTER SUMP 2" MIN ABOVE HIGHEST PRODUCT PENETRATION.
- 2 LOCATE ELECTRICAL JUNCTION BOX IN SUMP RISER POSITION AT HIGHEST LOCATION IN SUMP RISER FOR EASY ACCESS FROM ABOVE.
- 3 SUMP SENSOR TO REST AT LOWEST POINT OF SUMP ENCLOSURE ON PRODUCT PIPING SIDE OF SUMP. TANK CONTRACTOR TO SUPPLY ALL BRACKETS AND CLAMPS FOR 2-POINT MOUNTING OF THE 2" FRP SLEEVE.
- 4 SET MANHOLE COVERS 1" ABOVE THE FINISH GRADE AND SLOPE CONCRETE AWAY FROM MANWAY COVERS AT 2% GRADE.
- 5 POSITION BALL VALVE FOR UNOBSTRUCTED USE OF SHUT-OFF HANDLE.
- 6 STP POWER CONDUITS MUST BE RUN IN SEPARATE RACEWAY. ENTIRE WIRING RUN MUST BE IN RIGID PVC COATED GALVANIZED STEEL CONDUIT. USE ONLY ROB ROY CONDUIT TYPES "PERMA-COAT", "PLASTI-BOND" OR "KORKAP". NO PVC OR RNC CONDUIT ALLOWED.
- 7 USE ONLY ROB ROY CONDUIT TYPES "PERMA-COAT", "PLASTI-BOND" OR "KORKAP" ELECTRICAL CONDUITS AT FIBERGLASS TANK SUMPS OR WHERE BRAVO F-17-RR OR F-10-RR ENTRY FITTINGS ARE CALLED OUT.

Description
Date
Rev. #
Proto 2022-04

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
TANK SUMP DETAILS
DUAL OUTPUT



CORE STATES GROUP
CONCRETE STATES
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 092855
46 East Main Street
Suite 200A, NJ 08876
908.462.9700
core-states.com

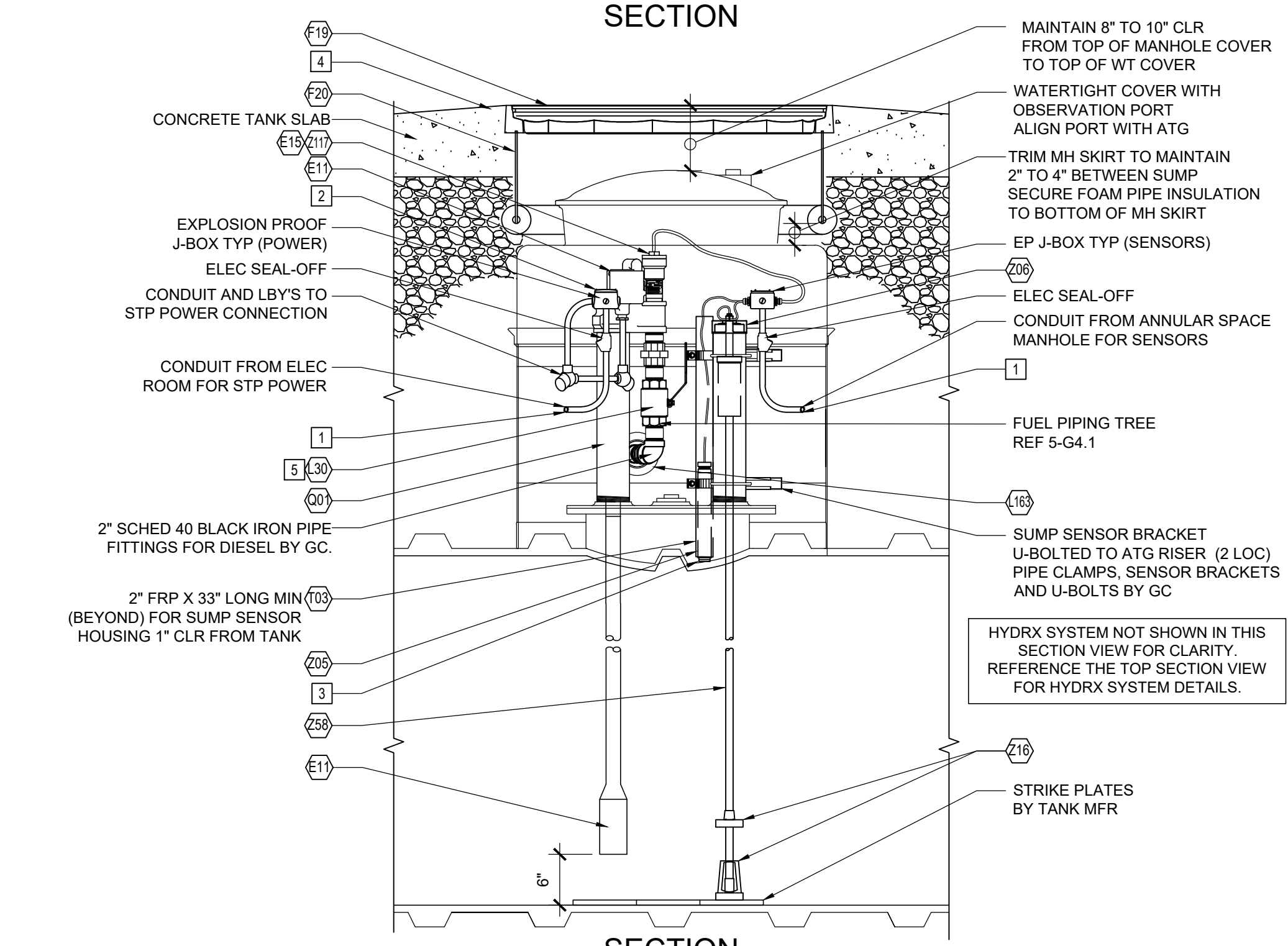
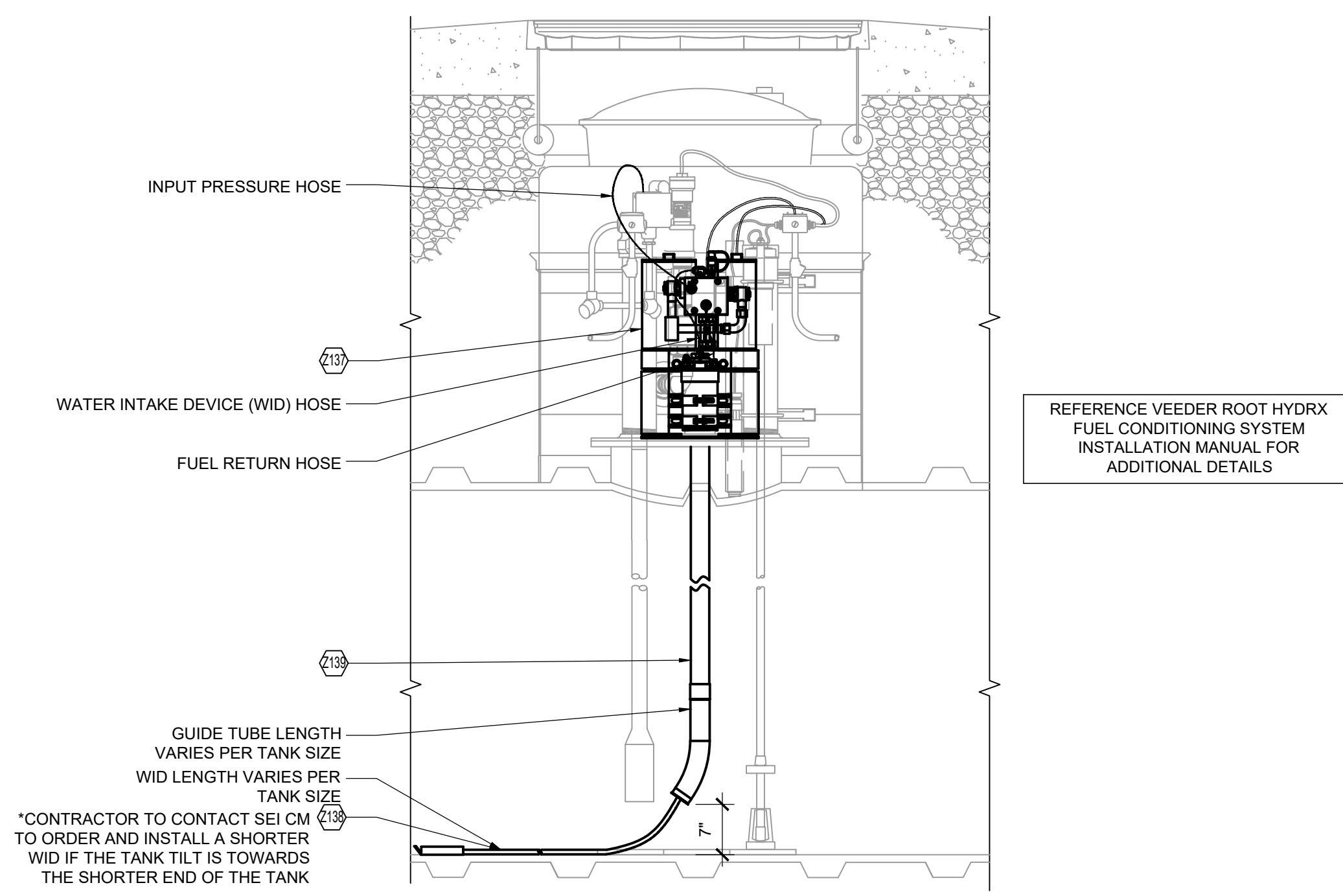
Job#: SPW-36100-SEI
Scale: AS NOTED
Date: 02/14/23
Drawn By: RMC
Checked By: CGC

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09-19-2023

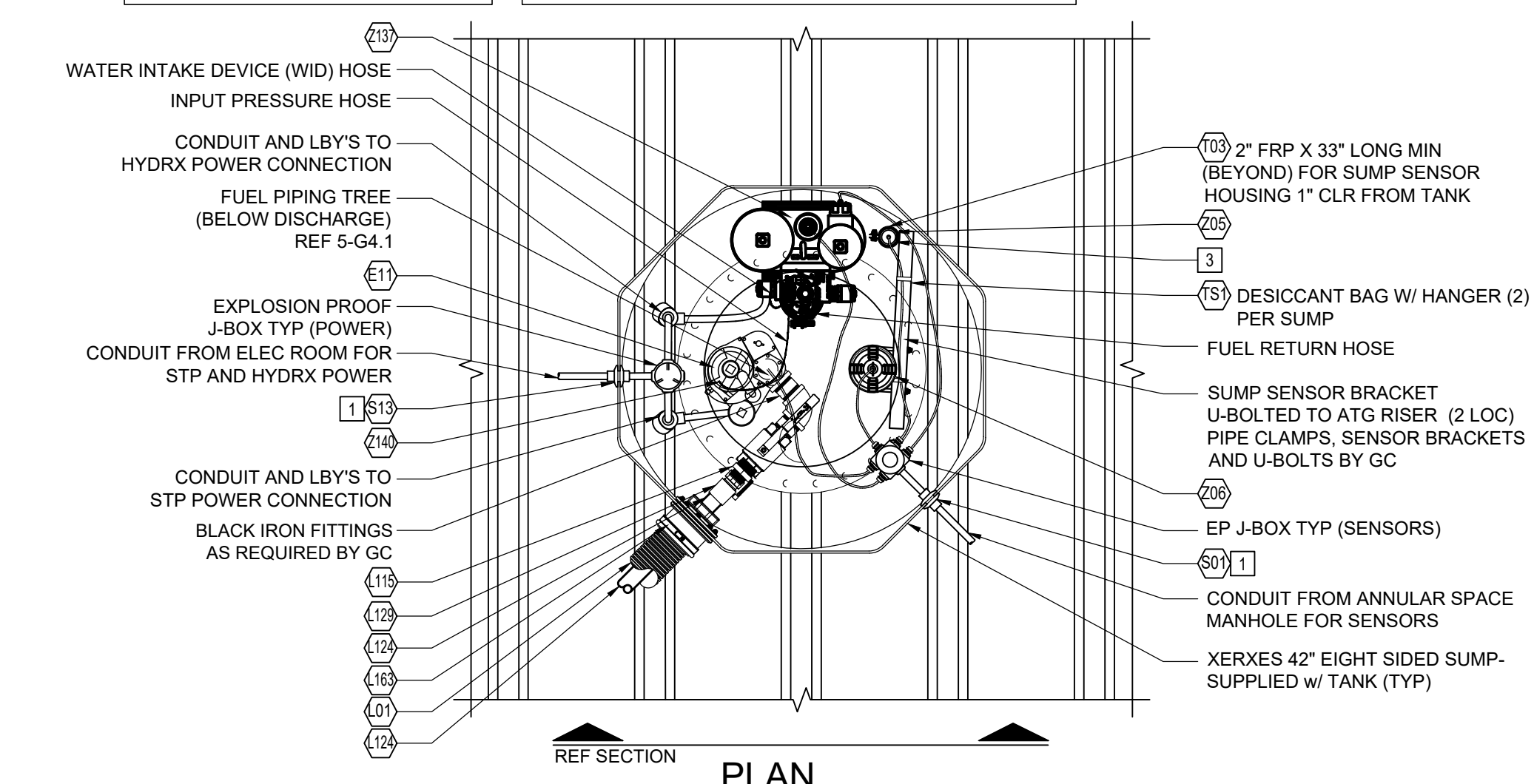
SHEET:
G4.0
FUELING - USA/D/O



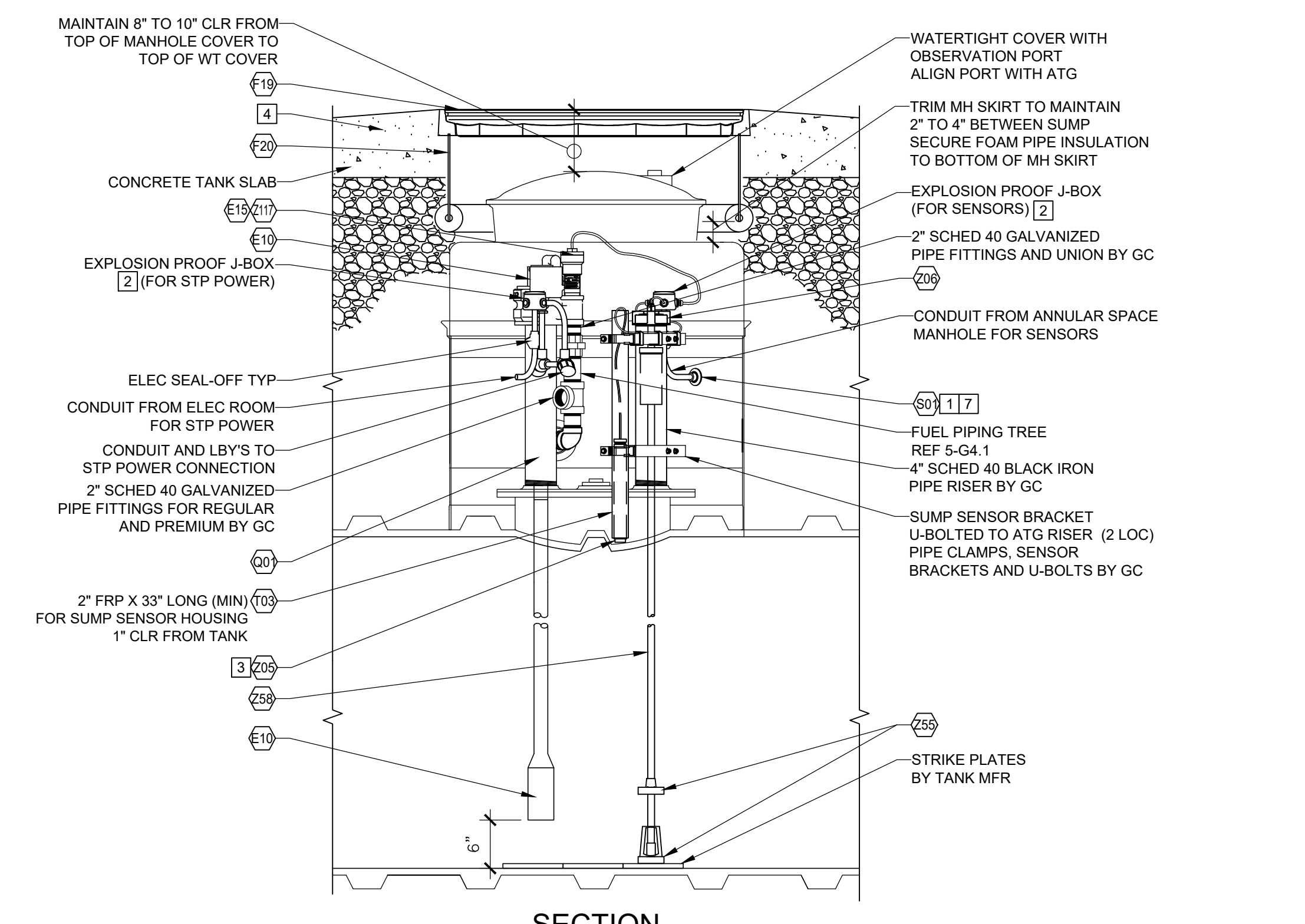
ROTATE VIEW AS REQUIRED FOR FIELD CONDITION

PIPING PENETRATIONS INTO SUMP ARE SHOWN FOR REFERENCE ONLY - COORDINATE LOCATIONS WITH SHEET G0.1 AND FIELD CONDITIONS

ELECTRICAL PENETRATIONS INTO SUMP ARE SHOWN FOR REFERENCE ONLY - COORDINATE LOCATIONS WITH SHEET G9.0 AND FIELD CONDITIONS



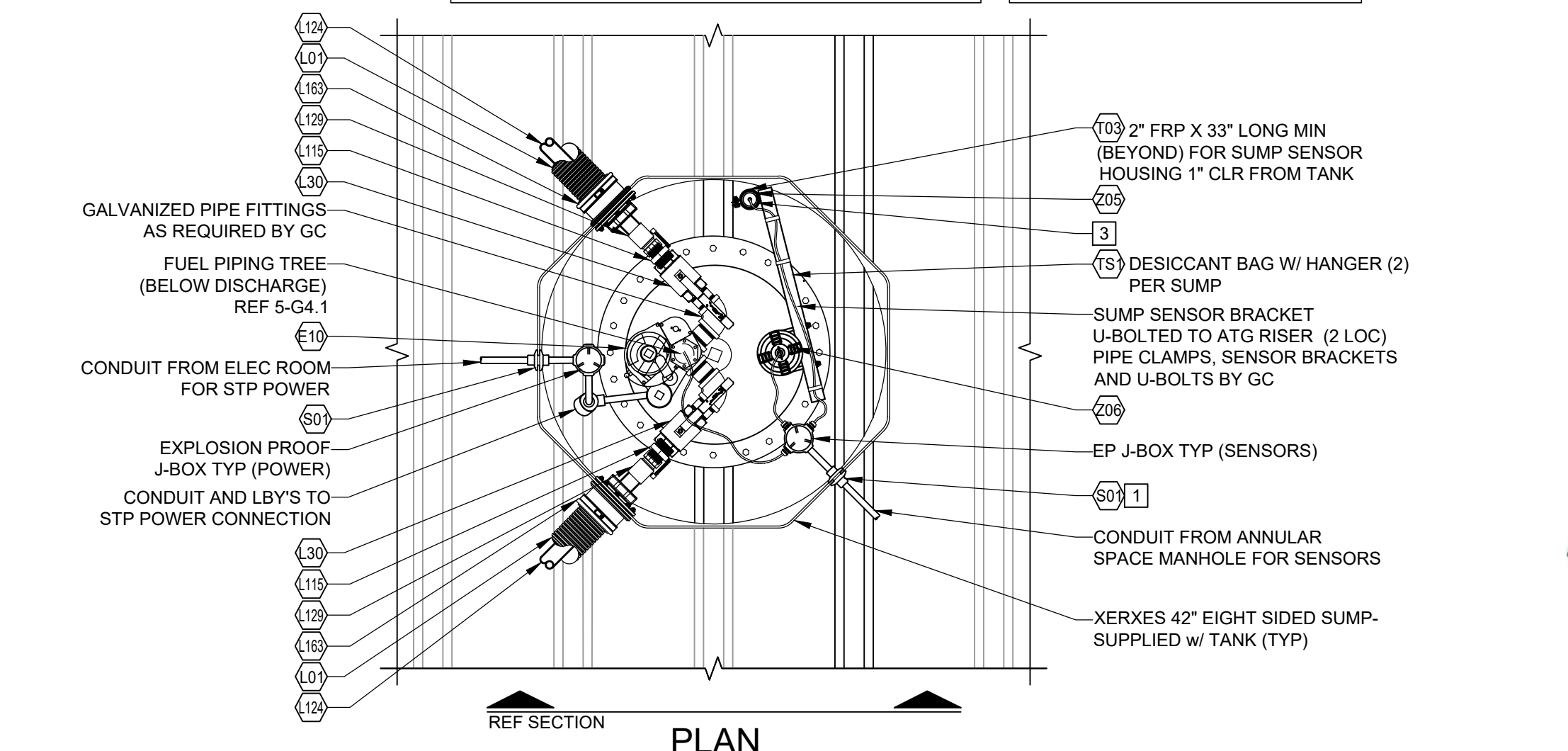
2 DIESEL STP SUMP DETAILS
3/4"=1'-0"



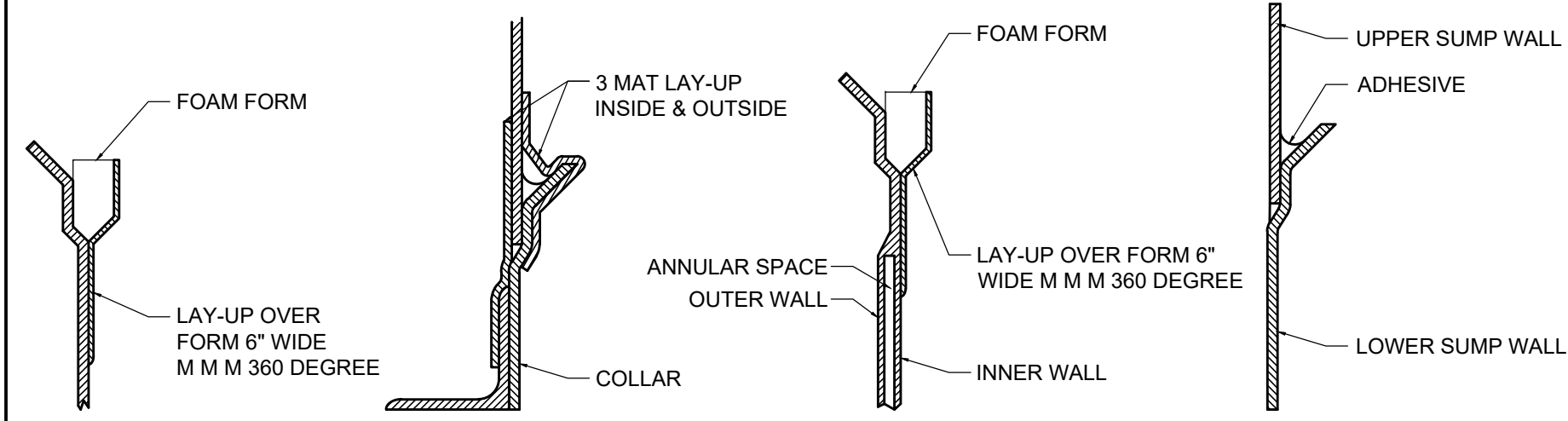
ROTATE VIEW AS REQUIRED FOR FIELD CONDITION

PIPING PENETRATIONS INTO SUMP ARE SHOWN FOR REFERENCE ONLY - COORDINATE LOCATIONS WITH SHEET G0.1 AND FIELD CONDITIONS

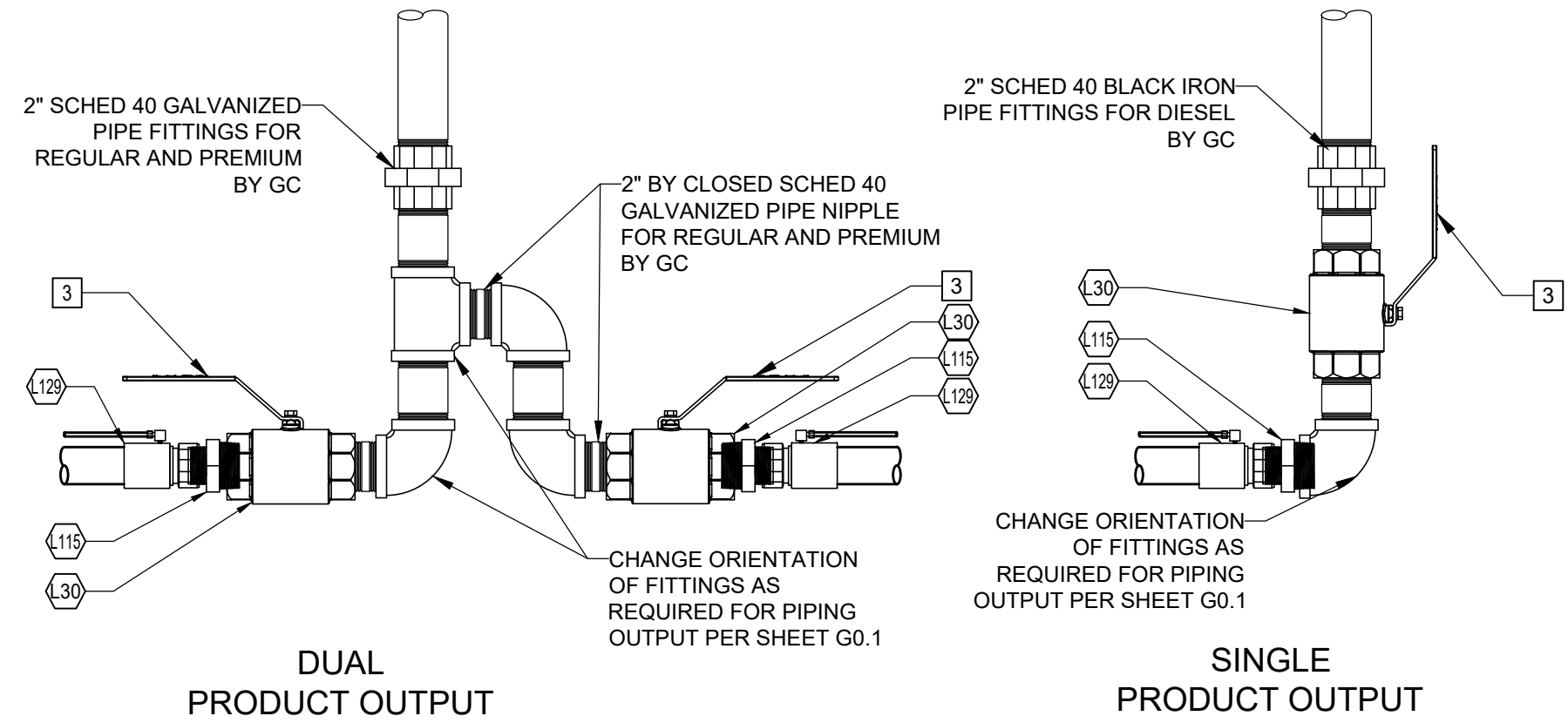
ELECTRICAL PENETRATIONS INTO SUMP ARE SHOWN FOR REFERENCE ONLY - COORDINATE LOCATIONS WITH SHEET G9.0 AND FIELD CONDITIONS



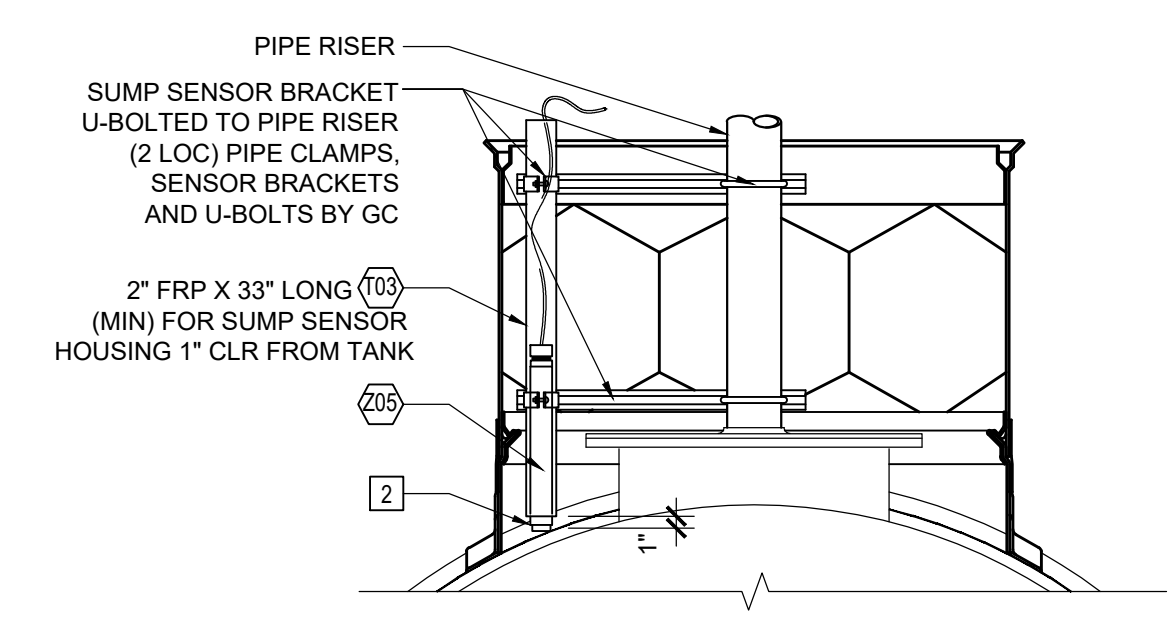
1 REGULAR AND PREMIUM STP SUMP DETAILS
3/4"=1'-0"



6 TANK SUMP LAYUP DETAILS
NTS



5 FUEL PIPING TREE DETAILS
1 1/2"=1'-0"



4 SUMP SENSOR AND SENSOR BRACKET DETAIL
3/4"=1'-0"

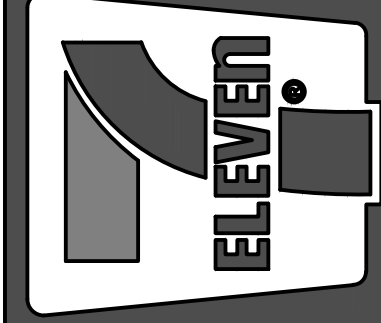
KEY NOTES

- CONCRETE SLAB, REF G0.1 FOR TOS ELEVATIONS. ALL MANHOLES COVERS WITHIN THE TANK SLAB AREA ARE TO BE SET 1" ABOVE THE FINISH GRADE AND SLOPE CONCRETE AWAY FROM MANWAY COVERS AT 2% GRADE. CROWN CONCRETE AS TO NOT INHIBIT WATER DRAINAGE OFF THE TANK SLAB (NO POOLING).
- SUMP SENSOR TO REST AT LOWEST POINT OF SUMP ENCLOSURE ON PRODUCT PIPING SIDE OF SUMP. TANK CONTRACTOR TO SUPPLY UNISTRUT BRACKETS, PIPE CLAMPS AND 2" FRP SLEEVE FOR 2-POINT MOUNTING OF THE SLEEVE.
- POSITION BALL VALVE FOR UNOBSTRUCTED USE OF SHUT-OFF HANDLE.
- OVERFILL PREVENTION VALVE INSTALLED PER MFR'S SPECIFICATIONS. SET OVERFILL DEVICE FOR RESTRICTION OF FLOW AT OR BELOW 90% TANK CAPACITY AND POSITIVE SHUT-OFF AT 95% TANK CAPACITY TO MEET NFPA 30 REQUIREMENTS

Rev. #	Date	Description

Proto 2022-04

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
TANK SUMP DETAILS



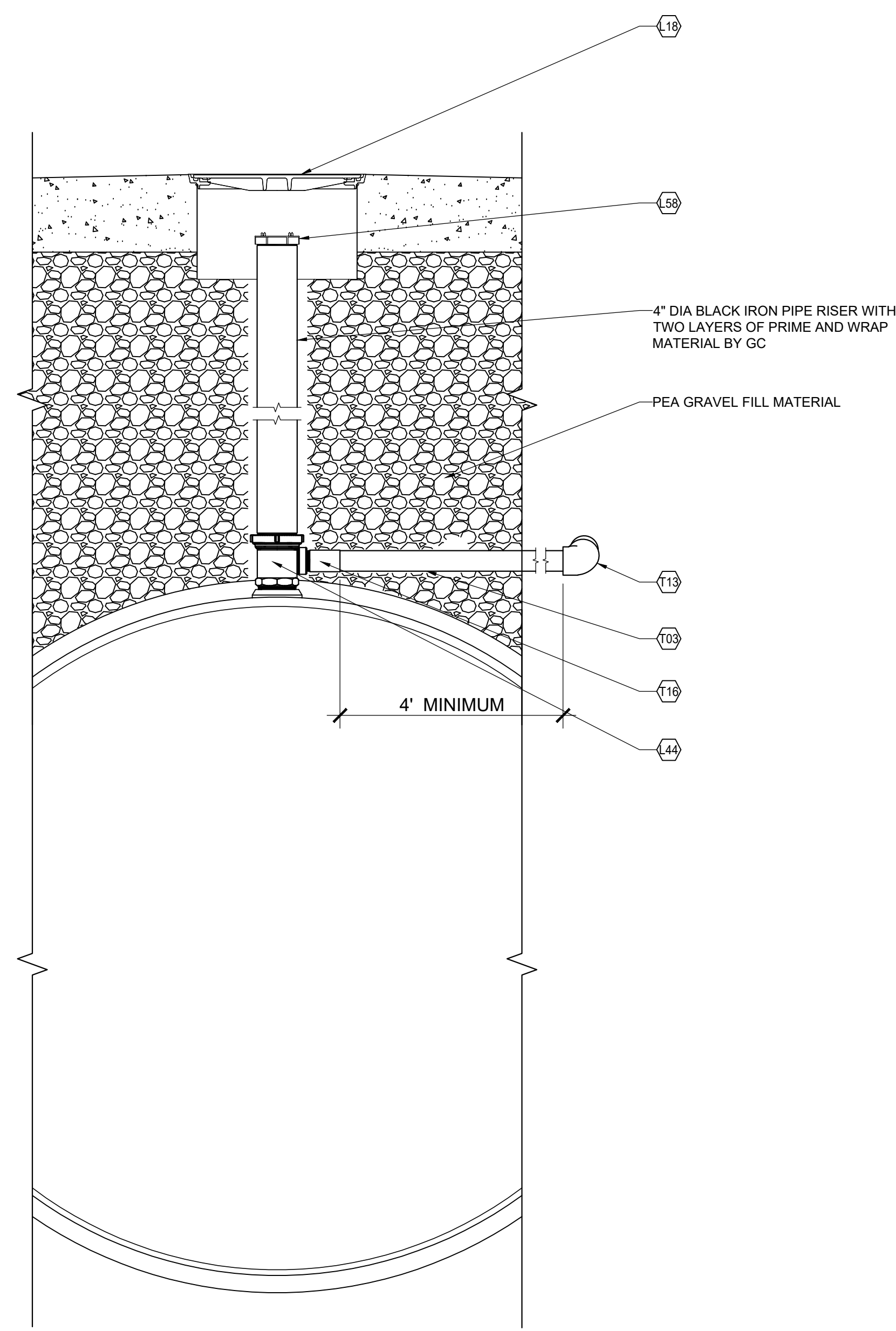
CORE STATES GROUP
ARCHITECTURE AND ENGINEERING P.C.
46 East Main Street
Suite 200
Spartanburg, SC 29576
803.462.9700
core-states.com

Job#: SPW-36100-SEI
Scale: AS NOTED
Date: 02/14/23
Drawn By: RMC
Checked By: CGC

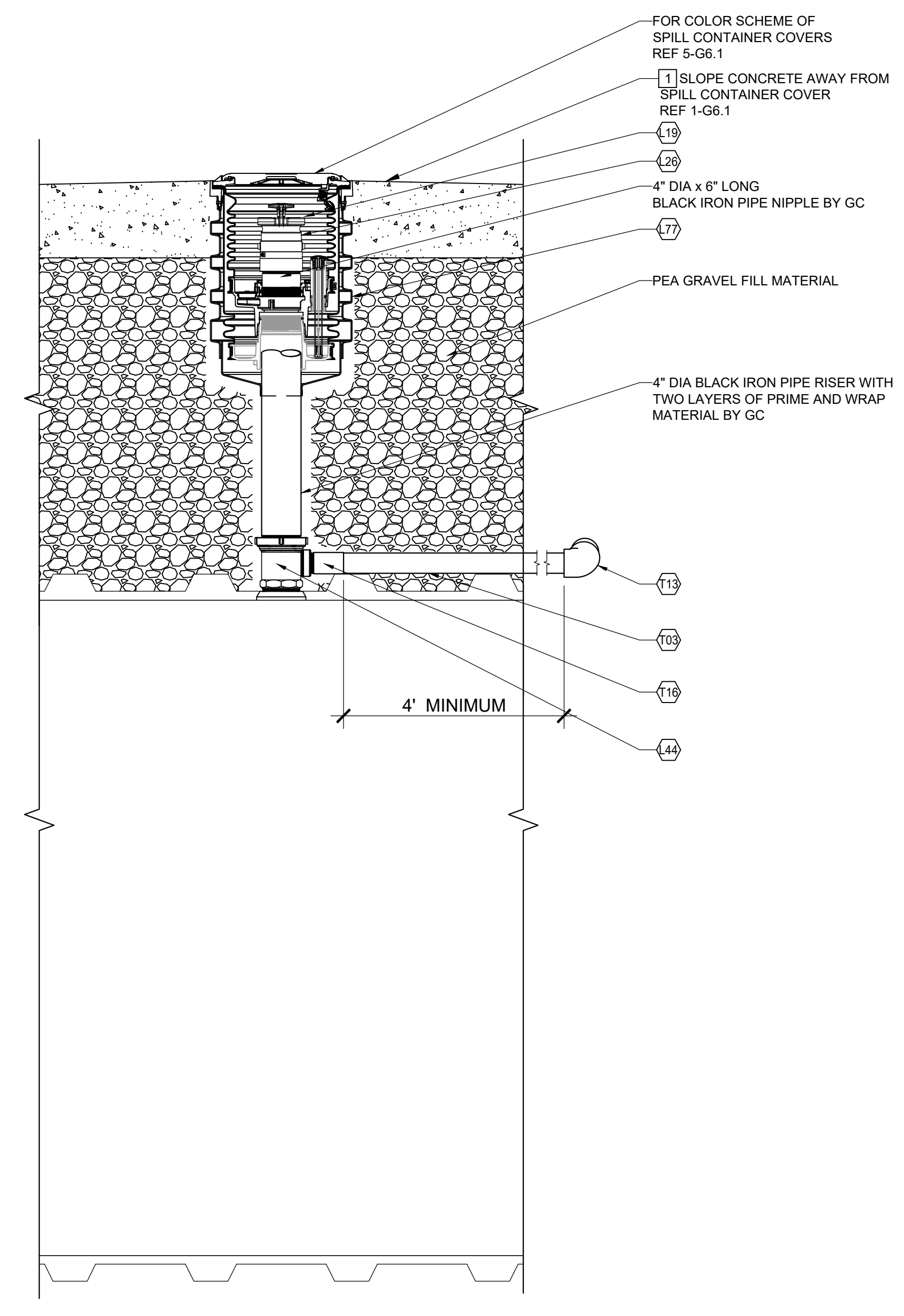
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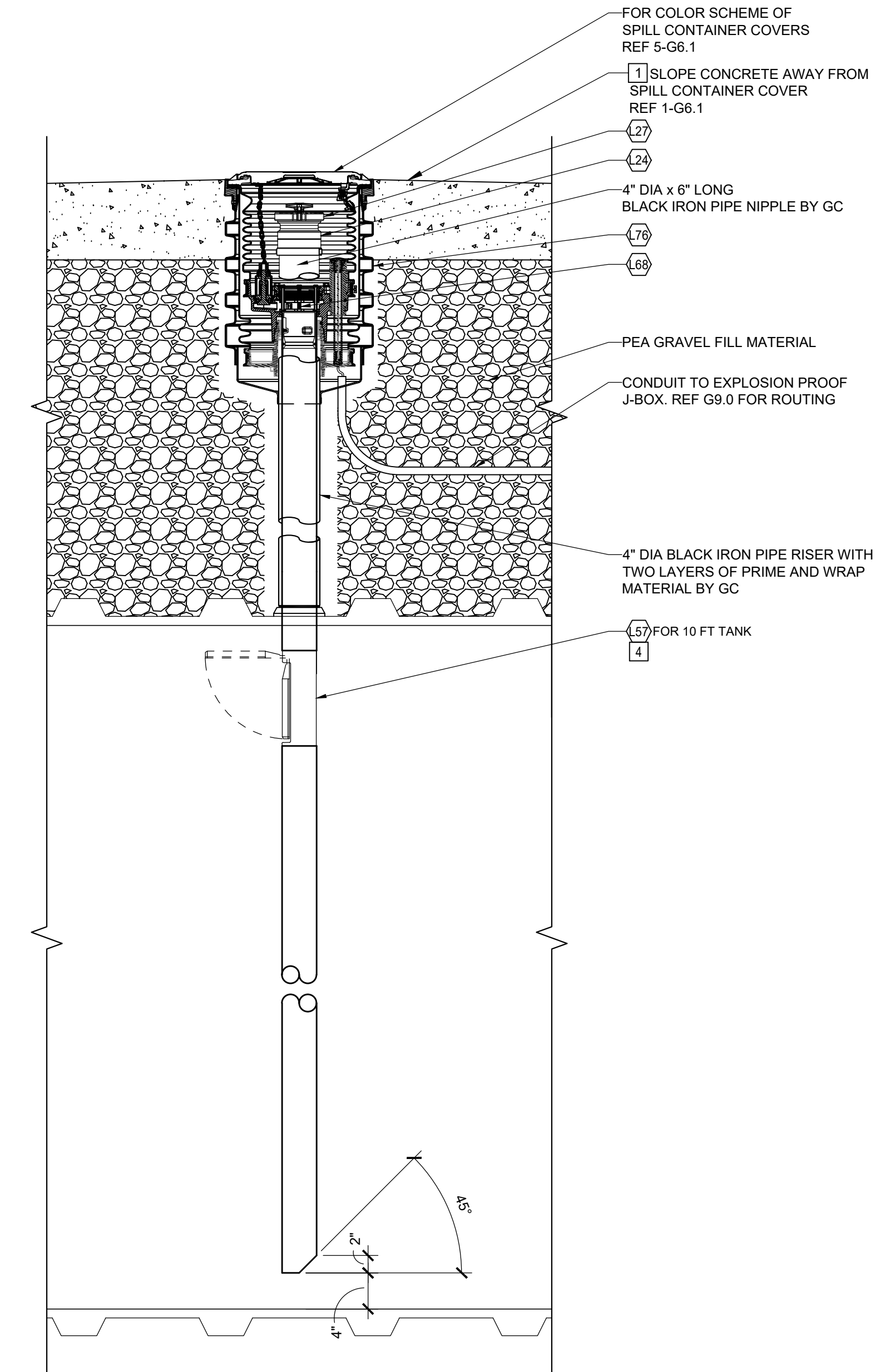
SHEET:
G4.1
FUELING - USA



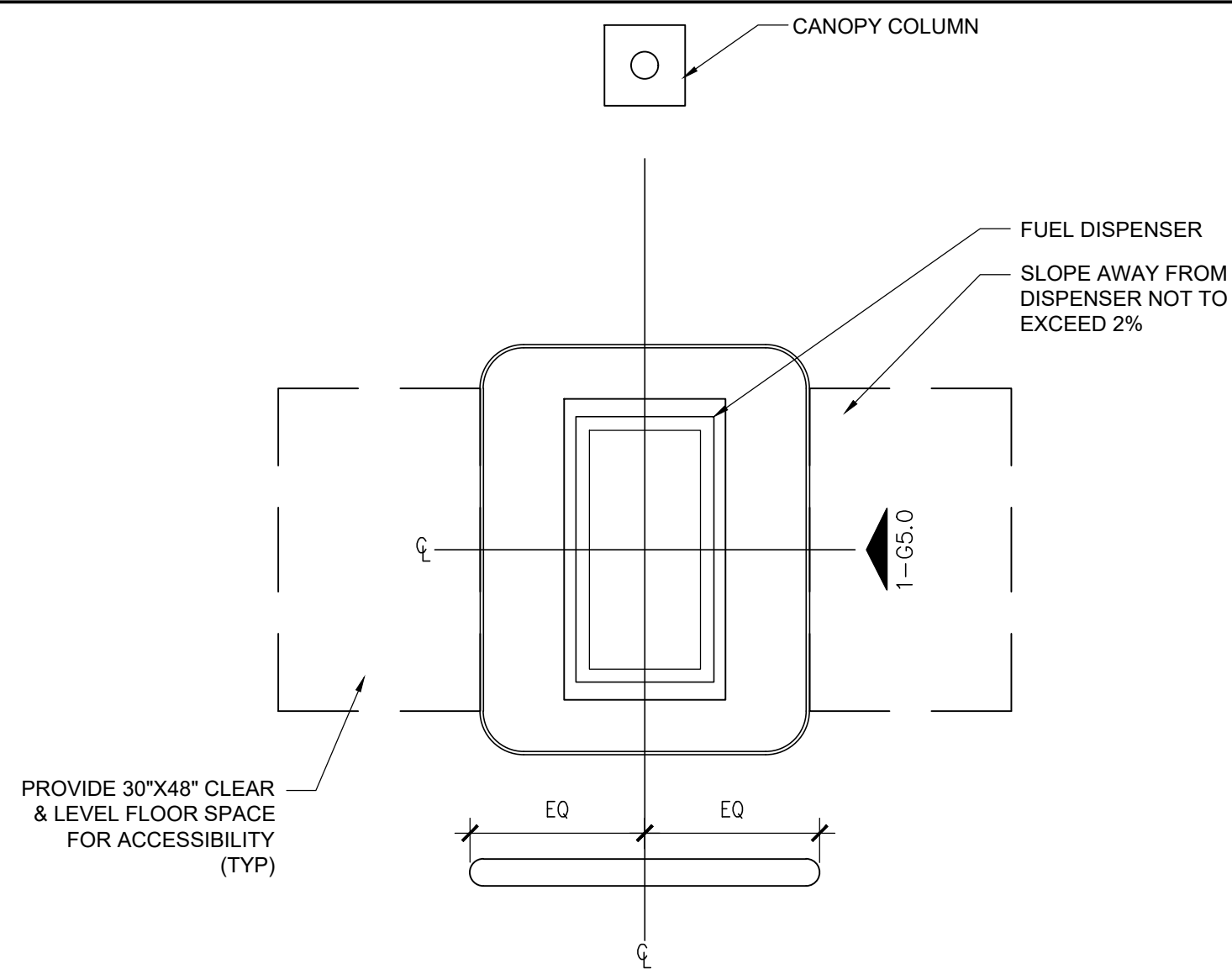
3 DIESEL VENT DETAIL
1"=1'-0"



2 RUL / PUL VENT/VAPOR DETAIL
1"=1'-0"



1 FUEL FILL CONTAINER DETAIL
1"=1'-0"



7 DISPENSER PLAN
1/2"=1'-0"

6 NOT USED
1/2"=1'-0"

5 MANHOLE COLOR SCHEDULE
1/4"=1'-0"

RUL E10	○	WHITE
PUL E10	●	RED
DSL <B5	●	YELLOW
VAPOR	●	ORANGE

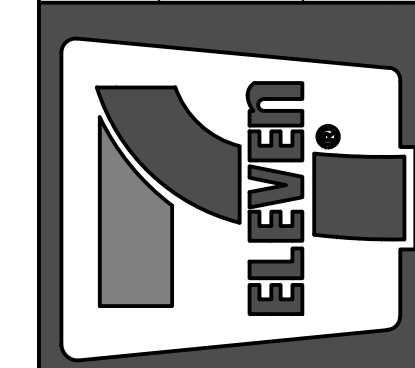
ALL OTHER SMALL MANHOLES PAINT BLACK

KEY NOTES

1	CONCRETE SLAB. REF G0.1 FOR TOS ELEVATIONS. ALL MANHOLES WITHIN THE TANK SLAB AREA TO BE RAISED 1" ABOVE THE NORMAL GRADE OF CONCRETE. SLOPE CONCRETE TO MANHOLES 1/4" PER FT. CROWN CONCRETE AS TO NOT INHIBIT WATER DRAINAGE OFF THE TANK SLAB (NO POOLING).
---	--

Description	Date	Rev. #

Proto 2022-04
7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
FUELING DIMENSIONAL PLAN



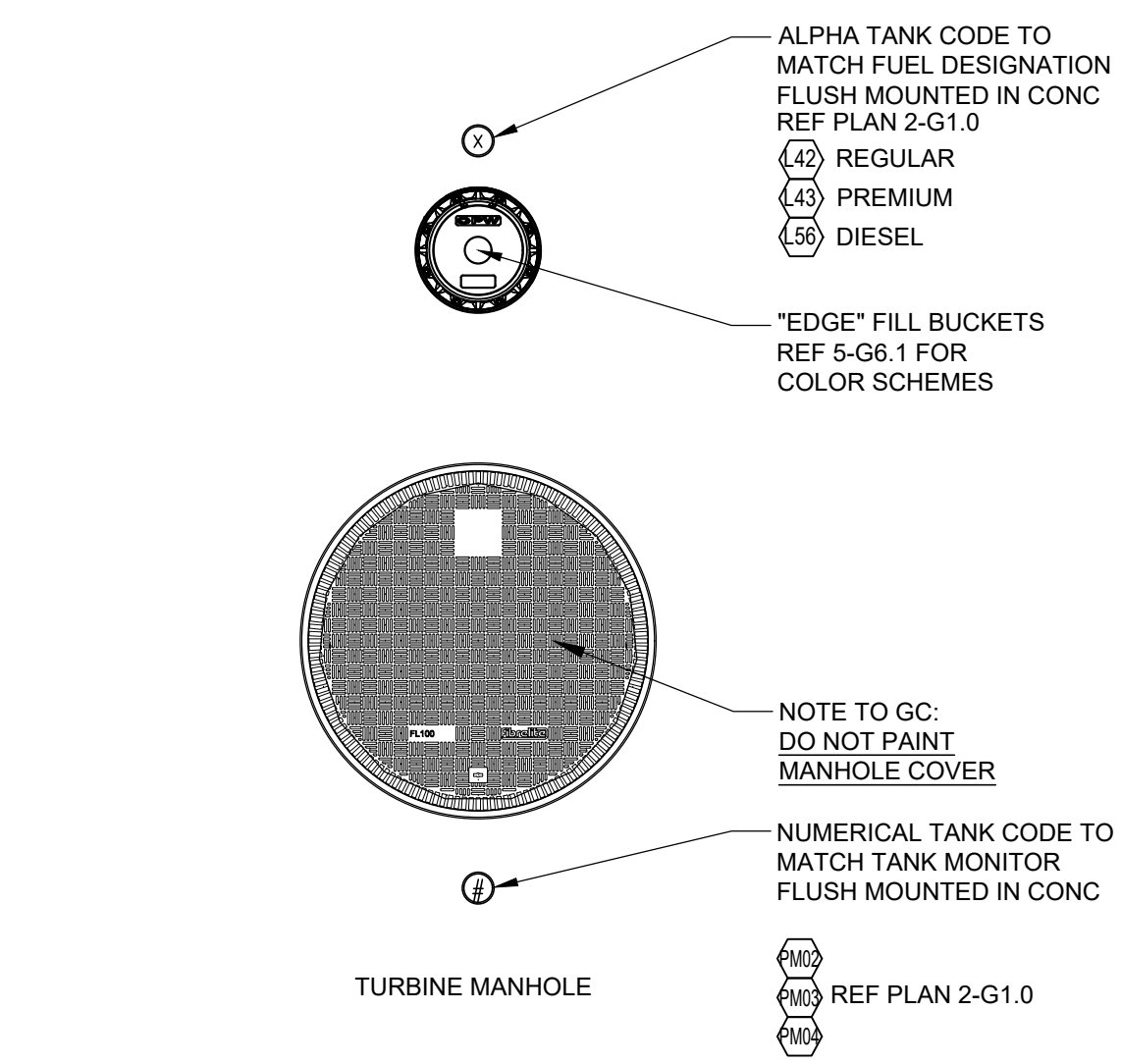
CORE STATES GROUP
CONCRETE ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION NO. 092855
46 East Main Street
Suite 200A, NJ 08876
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core-states.com

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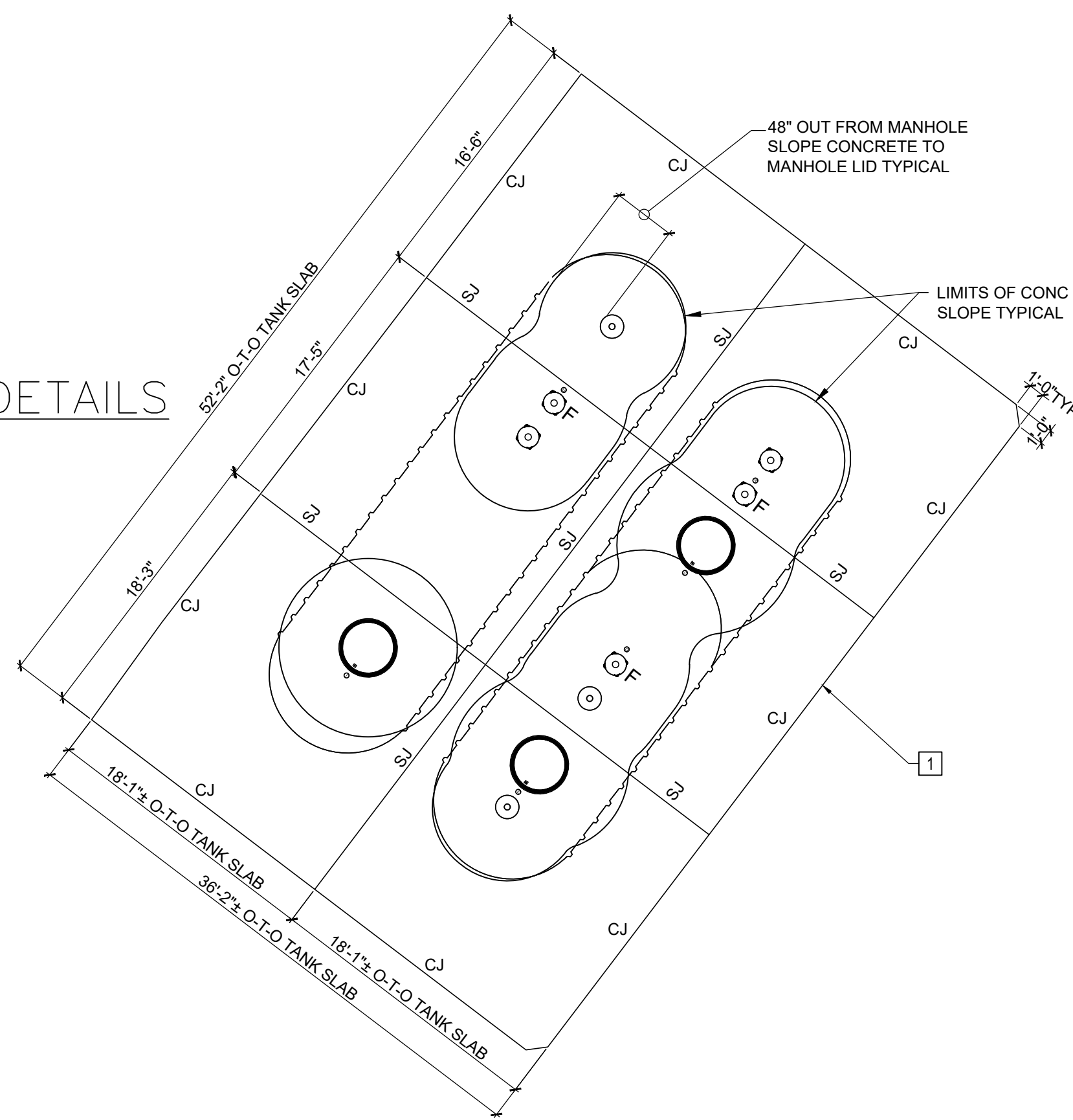
SHEET: G6.1
FUELING - USA



4 CONCRETE TAG AND MARKER DETAILS
1/2"=1'-0"

3 NOT USED
1/8"=1'-0"

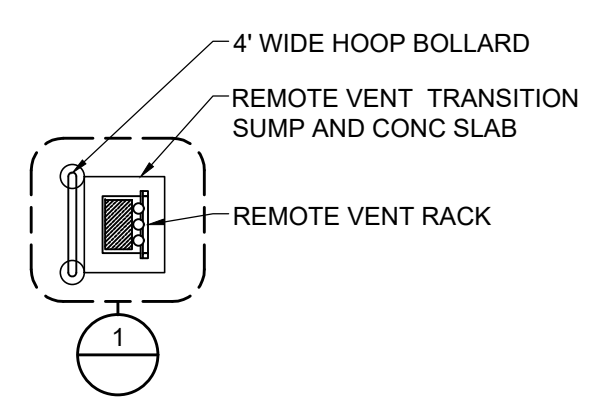
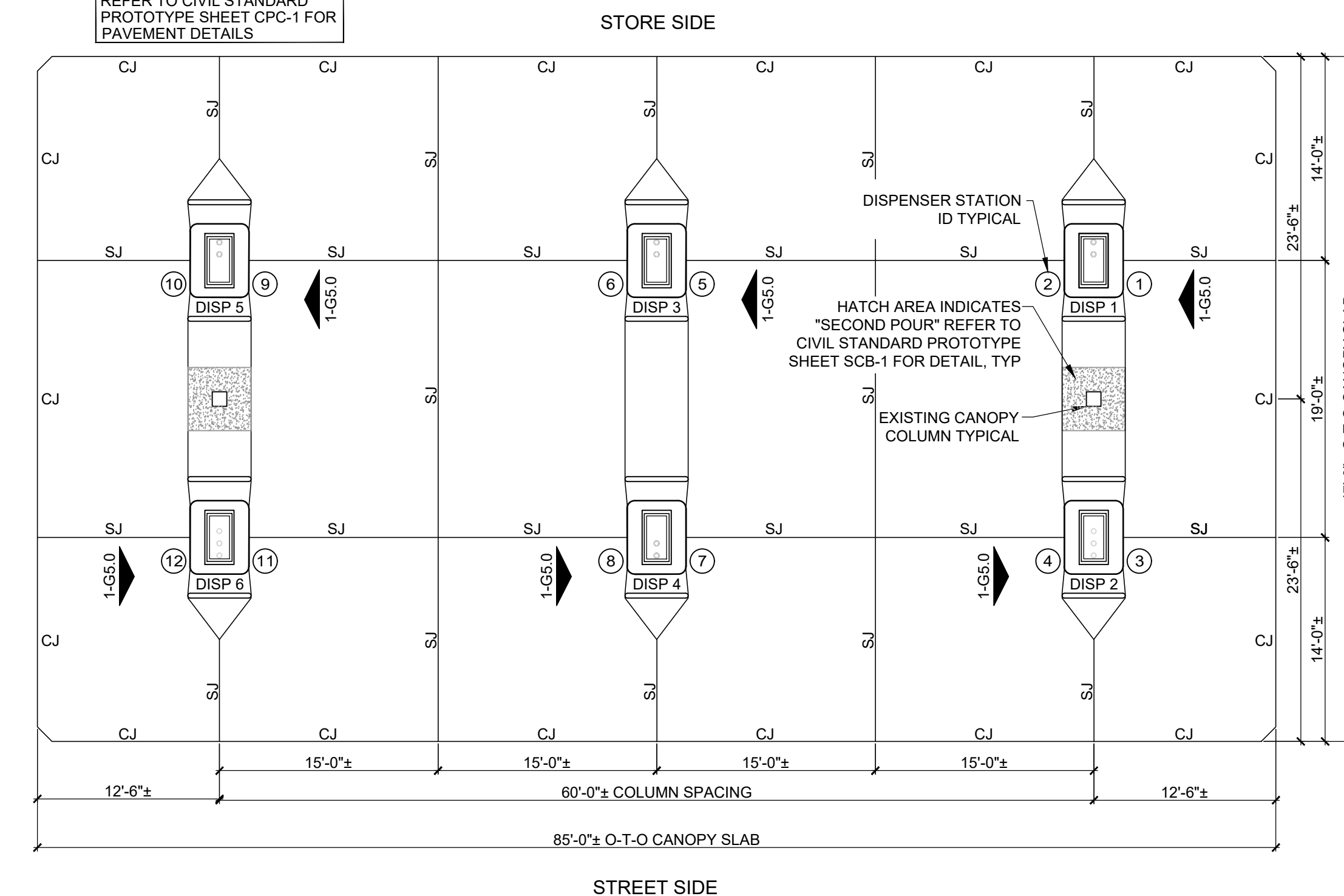
2 NOT USED
3/8"=1'-0"



1 FUEL AREA SLAB LAYOUT AND DIMENSIONAL PLAN
1/8"=1'-0"

NOTE:
ALL TOP OF MANHOLE RINGS TO BE 1" ABOVE GRADES. REFERENCE SHEET G0.1 FOR GRADING. SLOPE CONCRETE 1/4" PER FT TO NORMAL GRADES TYPICAL.

NOTE:
REFER TO CIVIL STANDARD PROTOTYPE SHEET CPC-1 FOR PAVEMENT DETAILS



3M SCHEDULE (CONTACT: MARK HODGES - MHODGES@SOURCENA.COM/877896-4328 FOR QUANTITY)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
A04	1	D-2400 PERFORMANCE SERIES WIRED INTERCOM 1 CHANNEL 8-STATION	78-6911-4796-7	06005142	3M	7-ELEVEN / SOURCE NA	GC
A05	1	I/O CARD 4 STATION	78-8117-3900-8	06005143	3M	7-ELEVEN / SOURCE NA	GC

FE PETRO SCHEDULE (CONTACT: MIKE FRISK - FRISK@FRANKLINFUELING.COM/512-850-7954 FOR QUANTITY)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
E01	2	MAG VARIABLE FREQUENCY CONTROLLER	MAG-VFC	06006133	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC
E04	1	CONTROL BOX W/ SWITCH	STP-CBS	06006137	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC
E10	2	2 HP VARIABLE SPEED SUBMERSIBLE PUMP W/ INTAKE FILTER	STPKVS2-VL2	03126506	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC
E11	1	1.5 HP FIXED SPEED SUBMERSIBLE PUMP W/ INTAKE FILTER	STPK150-VL2	03126507	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC
E15	3	CHECK VALVE TYPE "R"	400988932	03185395	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC
E17	3	DISPENSER HOOK ISOLATION BOX	STP-DHI	03430256	FE PETRO INC.	7-ELEVEN / SOURCE NA	GC

FIBRELITE/OPW SCHEDULE (CONTACT: PARRISH EVANS - PARRISH.EVANS@OPWGLOBAL.COM/706-202-7812)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
F19	3	40" SUBMERGED PUMP MANHOLE W/O SNOW FLOW RING	FL100BLACK-FC-KIT	03124662	FIBRELITE/OPW	7-ELEVEN / SOURCE NA	GC
F20	3	SKIRT FOR FL100	FL100-SK18	03124663	FIBRELITE/OPW	7-ELEVEN / SOURCE NA	GC

FLEXING SCHEDULE (CONTACT: MARK HODGES - MHODGES@SOURCENA.COM/877896-4328 FOR QUANTITY)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
G02	3	2" MS X 2" F X 18 FIREFLEX FLEX CONNECTOR	FF20X18M346XF	03158182	FLEXING	7-ELEVEN / SOURCE NA	GC

GILBARCO SCHEDULE (CONTACT: RYAN SCHUMACHER@GILBARCO.COM/336-207-1433 FOR QUANTITY)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
J08	6	SPEEDWAY DISPENSER VALANCE	XXXXXX	XXXXXX	PROSIGN	7-ELEVEN / GILBARCO	GC
J117	4	ENCORE 700S 3+0 BLENDER NN1 - NO VR	XXXXXX	03153026	GILBARCO	7-ELEVEN / GILBARCO	GC
J118	2	ENCORE 700S 3+1 BLENDER+DSL NL1 - NO VR	XXXXXX	03153029	GILBARCO	7-ELEVEN / GILBARCO	GC

HANGING HARDWARE SCHEDULE (CONTACT: PARRISH EVANS - PARRISH.EVANS@OPWGLOBAL.COM/706-202-7812)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
K03	12	3/4" X 8' FLEXSTEEL HARDWALL HOSE	GY34X8MM	03124651	SOURCENA	7-ELEVEN / SOURCE NA	GC
K04	16	3/4" X 8' FLEXSTEEL WHIP HOSE	GY34X8MMWHIP	03124652	SOURCENA	7-ELEVEN / SOURCE NA	GC
K48	12	3/4" CONVENTIONAL NOZZLE	11BP-0400	03158382	OPW	7-ELEVEN / SOURCE NA	GC
K49	4	3/4" NOZZLE FOR DIESEL	11B-0100	03158387	OPW	7-ELEVEN / SOURCE NA	GC
K50	16	3/4" CONVENTIONAL BREAKAWAY	66REC-1000	03158383	OPW	7-ELEVEN / SOURCE NA	GC
K51	12	3/4" SPLASHGUARD FOR CONVENTIONAL NOZZLE	8BL-0400	03158389	OPW	7-ELEVEN / SOURCE NA	GC
K52	4	DIESEL SPLASHGUARD	8G-0100	03158390	OPW	7-ELEVEN / SOURCE NA	GC
K119	16	CONVENTIONAL NOZZLE HOSE SWIVEL	45-5060	03126565	OPW	7-ELEVEN / SOURCE NA	GC
K121	4	3/4" X 9' BLACK HARDWALL HOSE, MALE X MALE ENDS	XXXXXX	03124635	SOURCENA	7-ELEVEN / SOURCE NA	GC

OPW SCHEDULE (CONTACT: PARRISH EVANS - PARRISH.EVANS@OPWGLOBAL.COM/706-202-7812)							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
L01	1000'	4" ACCESS PIPE - 250 FT BOX	AXP-40-250	03158339	OPW	7-ELEVEN / SOURCE NA	GC
L11	2	2" PRESSURE VACUUM VENTING CAP	623V-2203	06158288	OPW	7-ELEVEN / SOURCE NA	GC
L18	3	18" MANHOLE ASSY (ANNULAR AND DIESEL VENT)	104A-1800WT	06006209	OPW	7-ELEVEN / SOURCE NA	GC
L19	2	VAPOR CAP	1711T-7085-EVR	06006180	OPW	7-ELEVEN / SOURCE NA	GC
L24	3	FILL SWIVEL ADAPTER	61SALP-1020-EVR	06006214	OPW	7-ELEVEN / SOURCE NA	GC
L26	2	VAPOR RECOVERY SWIVEL ADAPTER	61VSA-1020-EVR	06006221	OPW	7-ELEVEN / SOURCE NA	GC
L27	3	TOP SEAL CAP EVR	634TT-7085-EVR	06006226	OPW	7-ELEVEN / SOURCE NA	GC
L30	5	2" BALL VALVE	21BV-0200	03158393	OPW	7-ELEVEN / SOURCE NA	GC
L36	1	TANK TAG - INTERIOR SPILL BUCKET (REGULAR UNLEADED)	1TAG-1010	06006188	OPW	7-ELEVEN / SOURCE NA	GC
L38	1	TANK TAG - INTERIOR SPILL BUCKET (PREMIUM UNLEADED)	1TAG-3010	06006192	OPW	7-ELEVEN / SOURCE NA	GC
L39	2	EXTERIOR VAPOR RECOVERY TANK TAG	1TAG-7000	06006195	OPW	7-ELEVEN / SOURCE NA	GC
L40	-	CONCRETE TANK MARKER (TANK GAUGE #1)	106N-1100	06006170	OPW	7-ELEVEN / SOURCE NA	GC
L41	-	CONCRETE TANK MARKER (TANK GAUGE #2)	106N-2200	06006171	OPW	7-ELEVEN / SOURCE NA	GC
L42	1	CONCRETE TANK MARKER (REGULAR UNLEADED)	106U-0125	06006175	OPW	7-ELEVEN / SOURCE NA	GC
L43	1	CONCRETE TANK MARKER (PREMIUM UNLEADED)	106P-0150	06006174	OPW	7-ELEVEN / SOURCE NA	GC
L44	3	4x2 TEE EXTRACTOR FITTING	233-4420	03158332	OPW	7-ELEVEN / SOURCE NA	GC
L45	1	DIESEL ATMOSPHERIC VENT VALVE	23-0033	06005039	OPW	7-ELEVEN / SOURCE NA	GC
L54	1	TANK TAG - INTERIOR SPILL BUCKET (DIESEL)	1TAG-4010	06006194	OPW	7-ELEVEN / SOURCE NA	GC
L56	1	CONCRETE TANK MARKER (DIESEL)	106DL-0095	06006169	OPW	7-ELEVEN / SOURCE NA	GC
L57	3	4" DROP TUBE WITH DUAL OVERFLOW VALVE 5FT BURY - 10FT TANK	7ISO-410C	03158324	OPW	7-ELEVEN / SOURCE NA	GC
L58	1	DIESEL VENT PIPE CAP	116-7085	03185354	OPW	7-ELEVEN / SOURCE NA	GC
L68	3	JACK SCREW FOR CAST BASE SPILL CONTAINER	61JSK-44CB	03158161	OPW	7-ELEVEN / SOURCE NA	GC
L72	-	CONCRETE TANK MARKER (TANK GAUGE #3)	106N-3300	06006172	OPW	7-ELEVEN / SOURCE NA	GC
L76	3	DOUBLEWALL SPILL BUCKET w/ SENSOR (FILL)	1C-3132D-50	03124688	OPW	7-ELEVEN / SOURCE NA	GC
L77	2	DOUBLEWALL SPILL BUCKET (VAPOR)	1C-3112P	03115701	OPW	7-ELEVEN / SOURCE NA	GC
L115	5	MALE ADAPTER FITTING	SMA-1520	06006113	OPW	7-ELEVEN / SOURCE NA	GC
L124	1000'	1.5" DW FLEX PIPE - 250 FT BOX	C15A-250	03158338	OPW	7-ELEVEN / SOURCE NA	GC
L129	28	1.5" OPW SWIVEL DW PIPE COUPLING	DPC-2150A	03124598	OPW	7-ELEVEN / SOURCE NA	GC
L130	9	TEST JUMPER TUBE	TCT-2509	03124576	OPW	7-ELEVEN / SOURCE NA	GC
L131	10	TERMINATION TEST JUMPER TUBE	TTT-2536	03124575	OPW	7-ELEVEN / SOURCE NA	GC
L163	5	RIGID ENTRY FITTING 4 x 1.5"	REF-4015	03124570	OPW	7-ELEVEN / SOURCE NA	GC
L194	1	DISPENSER SUMP - ENCORE (3 IN/3 OUT)	DSLFR-15-1836-2R-123567	XXXXXXX	OPW	7-ELEVEN / SOURCE NA	GC
L195	3	DISPENSER SUMP - ENCORE (2 IN/2 OUT)	DSLFR-15-1836-2R-1256	XXXXXXX	OPW	7-ELEVEN / SOURCE NA	GC
L196	1	DISPENSER SUMP - ENCORE (3 IN/0 OUT)	DSLFR-15-1836-2R-567	XXXXXXX	OPW	7-ELEVEN / SOURCE NA	GC
L197	1	DISPENSER SUMP - ENCORE (2 IN/0 OUT)	DSLFR-15-1836-2R-12	XXXXXXX	OPW	7-ELEVEN / SOURCE NA	GC
L230	6	COMPLETE 4x5x13" STAINLESS STEEL ISLAND FORM	6013SS-SFR6W4L5	03430283	OPW	7-ELEVEN / SOURCE NA	GC

PEMCO SCHEDULE							
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	FURNISHED BY	INSTALLED BY
PM02	1	CONCRETE TANK MARKERS (#5)	031-5	06006234	PEMCO	7-ELEVEN / SOURCE NA	GC
PM03	1	CONCRETE TANK MARKERS (#6)	031-6	06006235	PEMCO	7-ELEVEN / SOURCE NA	GC
PM04	1	CONCRETE TANK MARKERS (#7)	031-7	06006236	PEMCO	7-ELEVEN / SOURCE NA	GC

Job#: SPW-36100-SEI

AS NOTED

Scale:

AS NOTED

Date:

02/14/23

Drawn By:

RMC

Checked By:

CGC

3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
 1542 ROUTE 9
 WAPPINGERS FALLS, NY 12590
 EQUIPMENT SCHEDULES

CORE STATES GROUP
 CONCRETES
 ARCHITECTURE AND ENGINEERING P.C.
 CERTIFICATE OF AUTHORIZATION
 NO. 092879

SHEET: **G7.0**

FUELING - USA

LIGHT FIXTURE SCHEDULE (CONTACT: GRAYBAR NATIONAL - 7-ELEVEN.NATIONAL@GRAYBAR.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
M22	-	CREE LED UNDER CANOPY LIGHTING	CPY250-B-DM-F-B-UL-WH-57K-WH-HZ	03291316	CREE	7-ELEVEN / GRAYBAR GC

MASTER LOCK SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
N01	2	SERIES 175 TUMBLER LOCK	175	03185351	MASTER LOCK	7-ELEVEN / SOURCE NA GC

MAY ADVERTISING SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
P20	6	PUMP TOPPER SIDE MOUNTED	CT-XL-SL	XXXXXXX	M&M	7-ELEVEN / SOURCE NA GC

PIPE RISER SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
Q01	2	7" THIN WALL PIPE RISER	484RISER	03185353	GENERAL PIPE MANUF.	7-ELEVEN / SOURCE NA GC

POWER INTEGRITY SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
R01	1	E-STOP SWITCH RED INCLUDES RED COVER WITH TWIST	IA-ESOCAT/RD	03158209	POWER INTEGRITY	7-ELEVEN / SOURCE NA GC
R02	1	DISPENSER DATA / LOW VOLTAGE DISCONNECT	DDS-A808E	03430287	POWER INTEGRITY	7-ELEVEN / SOURCE NA GC
R04	1	E-STOP (CASHIER CONTROL STATION WITH PROTECTIVE SHROUD)	IA-ESORS	XXXXXXX	POWER INTEGRITY	7-ELEVEN / SOURCE NA GC

S. BRAVO SYSTEM SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
S01	5	3/4" ROB ROY FITTING	F-17-RR-7-11	03115710	S. BRAVO SYSTEM INC.	7-ELEVEN / SOURCE NA GC
S03	1	VAPOR VENT RACK	RS500JP-3S	03158039	S. BRAVO SYSTEM INC.	7-ELEVEN / SOURCE NA GC
S05	1	FIBERGLASS VAPOR VENT REMOTE SUMP	B500JPF-3SB	03158049	S. BRAVO SYSTEM INC.	7-ELEVEN / SOURCE NA GC
S13	1	1" ROB ROY FITTING	F-10-RR-7-11	03158376	S. BRAVO SYSTEM INC.	7-ELEVEN / SOURCE NA GC

SMITH FIBERCAST VAPOR / VENT PIPING SCHEDULE (CONTACT: LISA BLASSENGAME - LISA.BLASSENGAME@NOV.COM/210-842-5777 FOR QUANTITY)						
ITEM NO.	LGTH	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
T01	25'	4" SECONDARY PIPING (USE 25 FT LENGTHS)	011040-069-2	06006331	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T03	525'	2" PRIMARY PIPING (USE 25 FT LENGTHS)	011020-069-2	06006328	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC

SMITH FIBERCAST SCHEDULE (CONTACT: LISA BLASSENGAME - LISA.BLASSENGAME@NOV.COM/210-842-5777 FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
T08	12	2" PRIMARY SLEEVE COUPLING	012020-101-8	06006255	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T13	17	2" PRIMARY 90 ELBOW	012020-360-4	06006261	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T16	6	2" BELL X M	012020-191-4	06006256	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T24	-	2" PRIMARY TEE	012020-410-4	06006262	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T28	-	2" 45 PRIMARY ELBOW	012020-310-4	06006260	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T29	16	DS-8069 9.03 ADHESIVE KIT	022990-033-9	06006288	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T31	4	4" BELL X MALE	012040-191-4	06006278	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC
T32	3	2" BONDED FITTING (SINGLE WALL)	012020-622-0	03158380	NOV (REDTHREAD IIA)	7-ELEVEN / SOURCE NA GC

FIRE EXTINGUISHER (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
U01	2	PORTABLE FIRE EXTINGUISHER	3020	03158046	POTTER ROEMER	7-ELEVEN / SOURCE NA GC
U02	2	FIRE EXTINGUISHER CABINET	105-20-RR-C-H	03185372	CATO	7-ELEVEN / SOURCE NA GC

TYPAR FILTER FABRIC SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
V01	4	FILTER FABRIC 12- 1/2" X 300'	TYPAR-3401	06006313	TYPAR	7-ELEVEN / SOURCE NA GC

U BOLT SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
W01	14	U-BOLT ENCORE	M007038002	03185374	MISCELLANEOUS PARTS	7-ELEVEN / SOURCE NA GC

UNITED SIGN SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
X01	1	EMERGENCY SHUT OFF SWITCH SIGN	U-MS-76	06005037	UNITED SIGN	7-ELEVEN / SOURCE NA GC

VEEDER ROOT SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
Z05	9	LIQUID SUMP SENSOR	794380-208	06005066	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z06	5	4" ANNULAR / PROBE CAP	330020-282	06005053	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z08	1	7-11 ISP KIT	330020-409	06005054	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z14	2	10 FT. INTERSTITIAL TANK SENSOR	794390-409	03158249	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z16	1	4" FLOAT KIT FOR DIESEL	846400-001	03158176	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z42	1	G-SITE INSTALL KIT	331063-100	03158202	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z55	2	PHASE II FLOAT KIT (GAS)	886100-000	03305136	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z58	3	10" 6" ATG PROBE	846397-110	03165574	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z106	1	TLS-450PLUS TOUCH SCREEN CONSOLE W/ PRINTER	860091-302	03430216	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z107	1	TLS-450PLUS APPLICATION SOFTWARE	333545-001	03430215	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z108	1	BIR/ACCUCART LI EDIM FACTORY INSTALLED FOR TLS-450PLUS	333149-001	03430211	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z109	2	SIXTEEN INPUT UNIVERSAL SENSOR / PROBE INTERFACE MODULE TLS-450 INSTALLED	332812-001	03430210	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z116	1	RISK MANAGEMENT LEAK DETECTION SOFTWARE FOR DPLD FOR TLS-450	332972-008	03430214	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z117	3	DIGITAL PRESSURIZED LINE LEAK DETECTOR W/O SWIFT CHECK VALVE TLS-450	859080-001	03430179	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z119	1	SINGLE RS-232 INTERFACE MODULE (COMM. SLOTS 1, 2, 3) FOR TLS-450PLUS	332866-001	03430219	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z124	1	UNIVERSAL INPUT / OUTPUT INTERFACE MODULE TLS-450 - INSTALLED	332813-001	03430212	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z132	1	CONTINUOUS STATISTICAL LEAK DETECTION TLS-450	332972-006	03430213	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z133	-	CDM SOFTWARE	349889-001	XXXXXXXX	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z137	1	HYDRX - 250D FUEL CONDITIONER KIT	342909-325	03430289	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z138	1	HYDRX - WATER INTAKE DEVICE (WID) FOR FIBERGLASS TANKS, 3 TUBES	860823-261	03430290	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z139	1	HYDRX - GUIDE TUBE, 128 INCHES, 10" DIAMETER TANK & 2.5 GAL SYSTEM	860780-128	03430292	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z140	1	HYDRX - KIT - INSTALLATION, FE PETRO	330020-874	03430284	VEEDER ROOT	7-ELEVEN / SOURCE NA GC
Z110	1	ADAPTER DB9	13852-01	03124638	VERIFONE	7-ELEVEN / SOURCE NA GC
Z141	1	10-FOOT SHIELDED RS-232 CABLE	13836-01	XXXXXXXX	VERIFONE	7-ELEVEN / SOURCE NA GC



XERXES SCHEDULE (CONTACT: RICK WHATLEY - RICK.WHATLEY@SHAWCOR.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
AB30	1	XERXES TANK 10' DIA. (20K) w/ (1) STP SUMP, 4" FILL & V/V FITTINGS	645-430-01	03156196	XERXES	7-ELEVEN / XERXES GC
AB31	1	XERXES TANK 10' DIA. (12K / 8K SPLIT TANK) w/ (2) STP SUMPS, 4" FILL & V/V FITTINGS	645-431-01	03156197	XERXES	7-ELEVEN / XERXES GC

SIGNAGE SCHEDULE (CONTACT: MARTY CLARK - MCLARK@PERFORMANCE-INK.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
AJ1	12	4" X 5" EMERGENCY SHUT OFF SIGN (OUTSIDE)	NG-2402	XXXXXXX	PERFORMANCE INK	GC GC
AJ2	1	4" X 4" PUMP NUMBERS 1-6 - WHITE NUMBERS/BLACK BACKGROUND	1-6 NG-28	XXXXXXX	PERFORMANCE INK	GC GC
AJ5	4	3.5" X 3.25" ULTRA LOW SULFUR HIGHWAY DECAL	XXXXXXX	XXXXXXX	PERFORMANCE INK	GC GC
AJ6	12	2.76" X 4.75" CONTAINS 10% ETHANOL DECAL	NG-80R	XXXXXXX	PERFORMANCE INK	GC GC
AJ7	12	2.75" X 2.75" GAS PUMPS ARE VIDEO MONITORED DECAL	NG41	XXXXXXX	PERFORMANCE INK	GC GC
AJ8	6	20" X 6.5" WARNING - NO SMOKING	NG193	XXXXXXX	PERFORMANCE INK	GC GC
AJ9	1	4" X 4" PUMP NUMBERS 7-12 - WHITE NUMBERS/BLACK BACKGROUND	NG-NG-24SEI	XXXXXXX	PERFORMANCE INK	GC GC
AJ10	4	4"X24" ALUMINUM FIRE EXTINGUISHER SIGN	NG-97SEI	XXXXXXX	PERFORMANCE INK	GC GC
AJ11	-	8"X12" ALUMINUM OVERFILL SIGN	M-58	00330547	NATIONAL GAS	7-ELEVEN GC
AJ12	12	OUTDOOR LITERATURE HOLDER (2 PER DISP)	XXXXXXX	03304838	HICORP	7-ELEVEN/SOURCE GC
AJ13	-	4"X 4" PUMP NUMBERS 13-18	NG-25SEI	XXXXXXX	PERFORMANCE INK	GC GC

UNIVERSAL VALVE COMPANY SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
H01	13	4' X 72" U-SHAPED BUMPER	1983-404972-7E	03430180	UNIVERSAL VALVE	7-ELEVEN / SOURCE NA GC

TRANS-SORB SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
TS1	6	DESICCANT BAG WITH HANGER (2 PER SUMP)	XXXXXXX	03430194	TRANS-SORB	7-ELEVEN / SOURCE NA GC

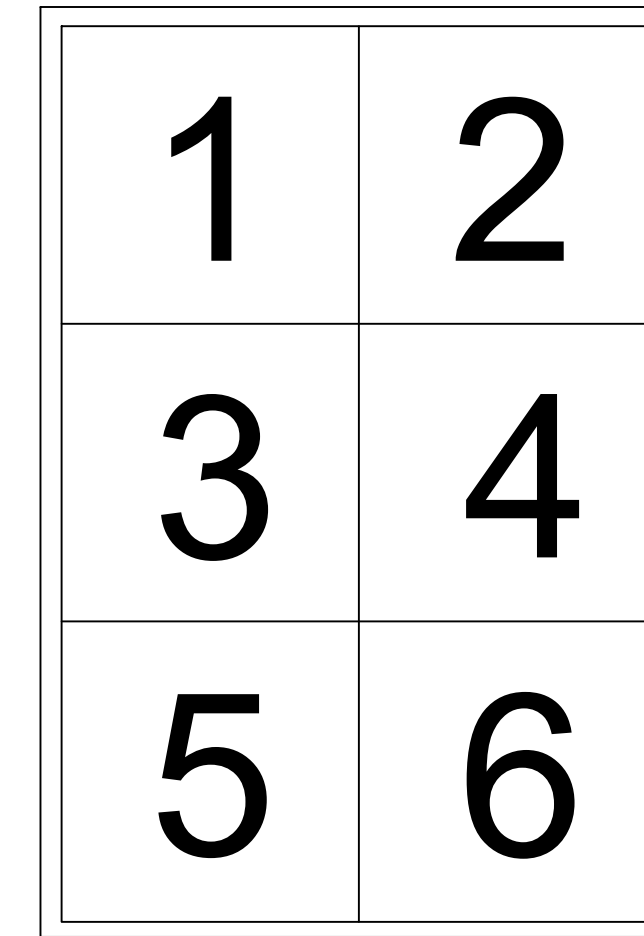
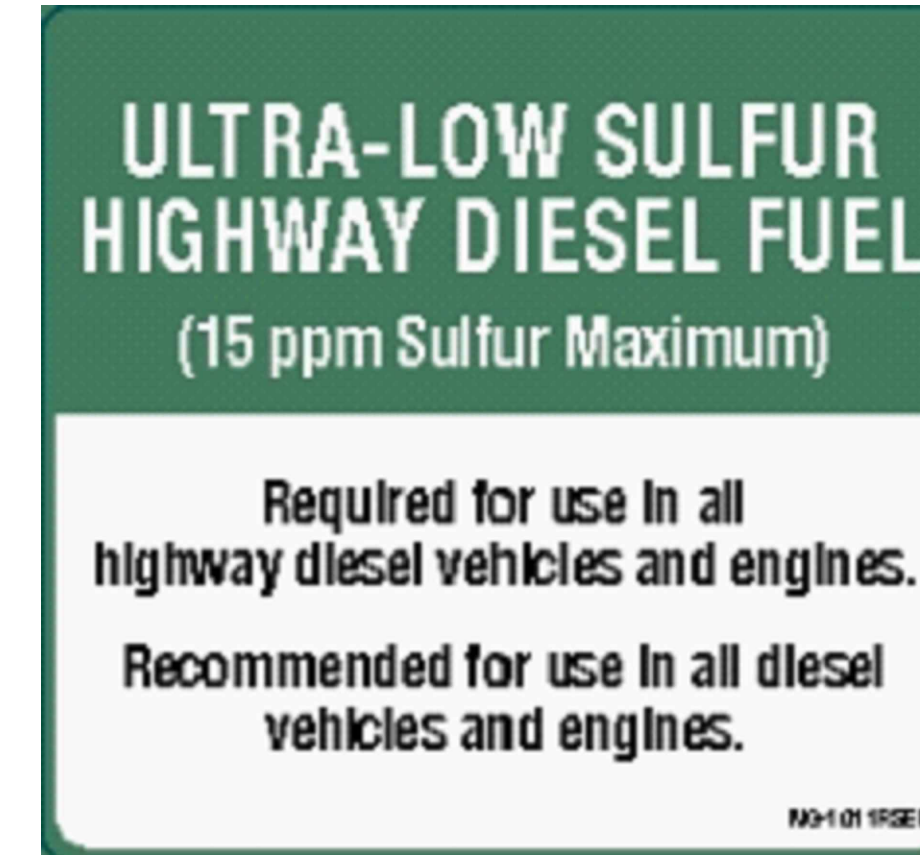
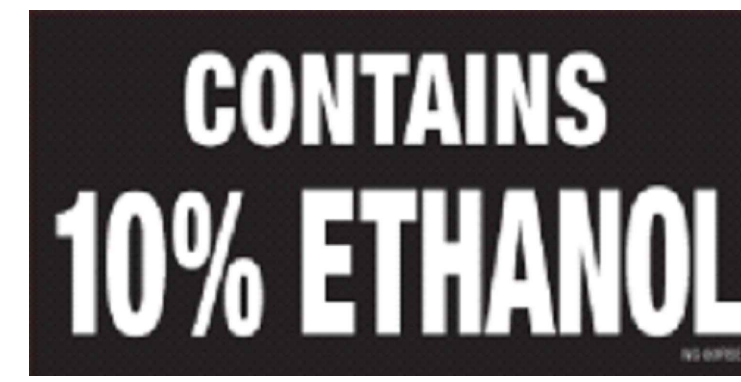
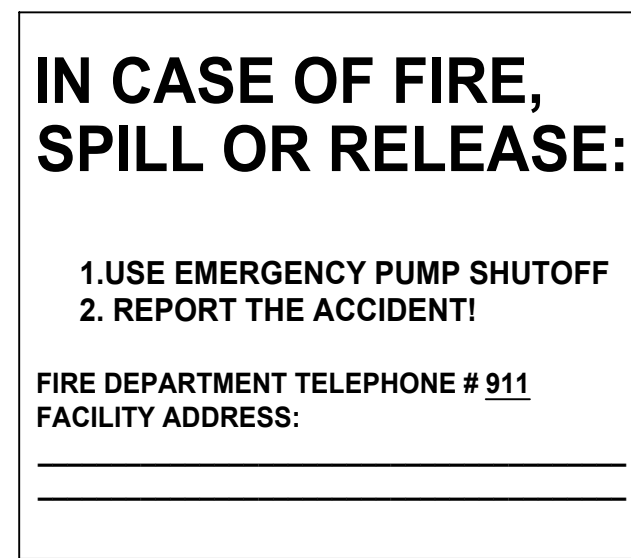
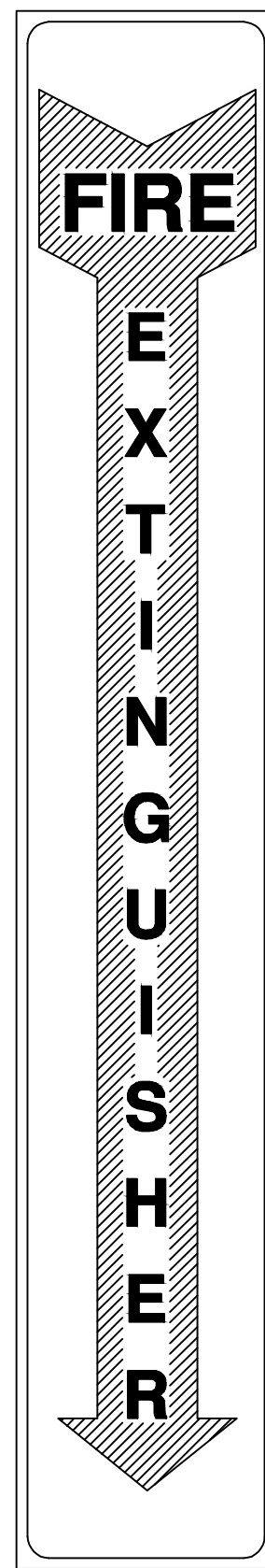
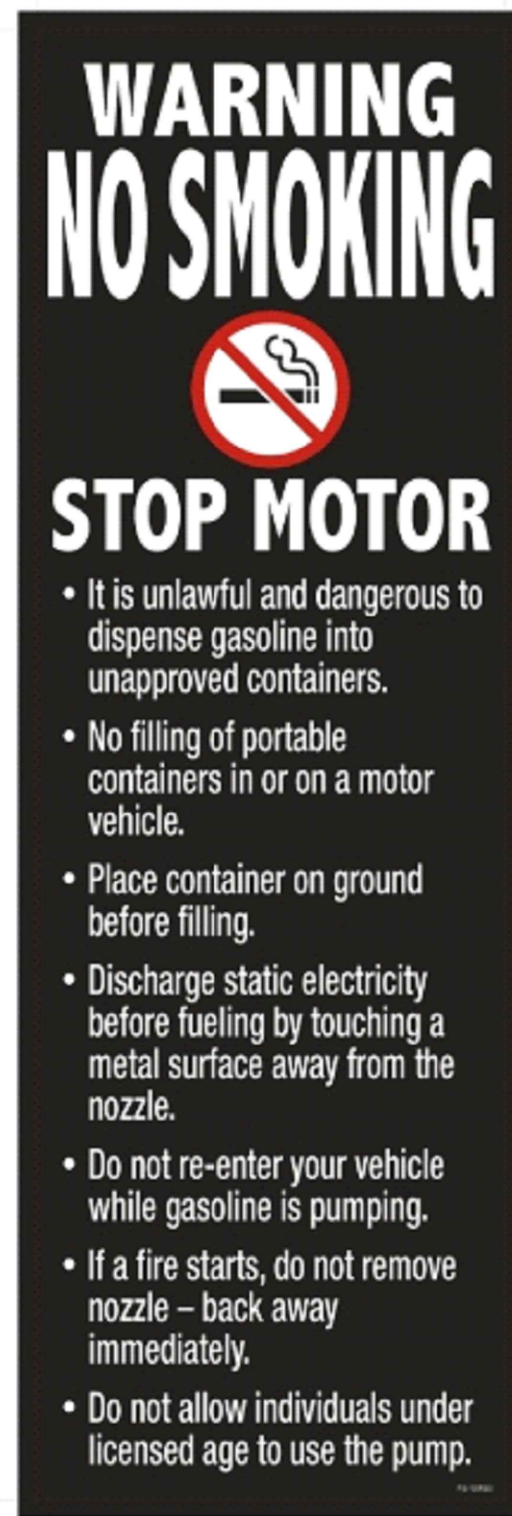
WINDY CITY WIRE SCHEDULE (CONTACT: SOURCE NORTH AMERICA - 7-ELEVEN SUPPORT TEAM - SNA711@SOURCENA.COM FOR QUANTITY)						
ITEM NO.	QTY.	DESCRIPTION	PART NO.	ORACLE NO.	MANUFACTURER	INSTALLED BY
WC1	2000'	GILBARCO DISPENSER COMMUNICATION CABLE (CAT6)	WCW7931AFEQ	03430209	WINDY CITY WIRE	7-ELEVEN / SOURCE NA GC

Job#: SPW-36100-SEI	Scale: AS NOTED	Date: 02/14/23	Drawn By: RMC	Checked By: CGC
 <p>CORE STATES GROUP CONCRETE ARCHITECTURE AND ENGINEERING P.C. CERTIFICATE OF AUTHORIZATION No. 092855</p> <p>46 East Main Street Sayreville, NJ 08876 908.462.9700 core-states.com</p>				
				
SHEET: G7.1				
FUELING - USA				

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GENERAL NOTES

- SIGNAGE IS PROVIDED BY 7-ELEVEN OR GC . SIGNAGE TO BE INSTALLED BY CONTRACTOR PER 7-ELEVEN GUIDELINES.



9 WARNING NO SMOKING N.T.S.

8 FIRE EXTINGUISHER N.T.S.

7 IN CASE OF FIRE, SPILL OR RELEASE N.T.S.

6 GAS PUMPS ARE VIDEO MONITORED N.T.S.

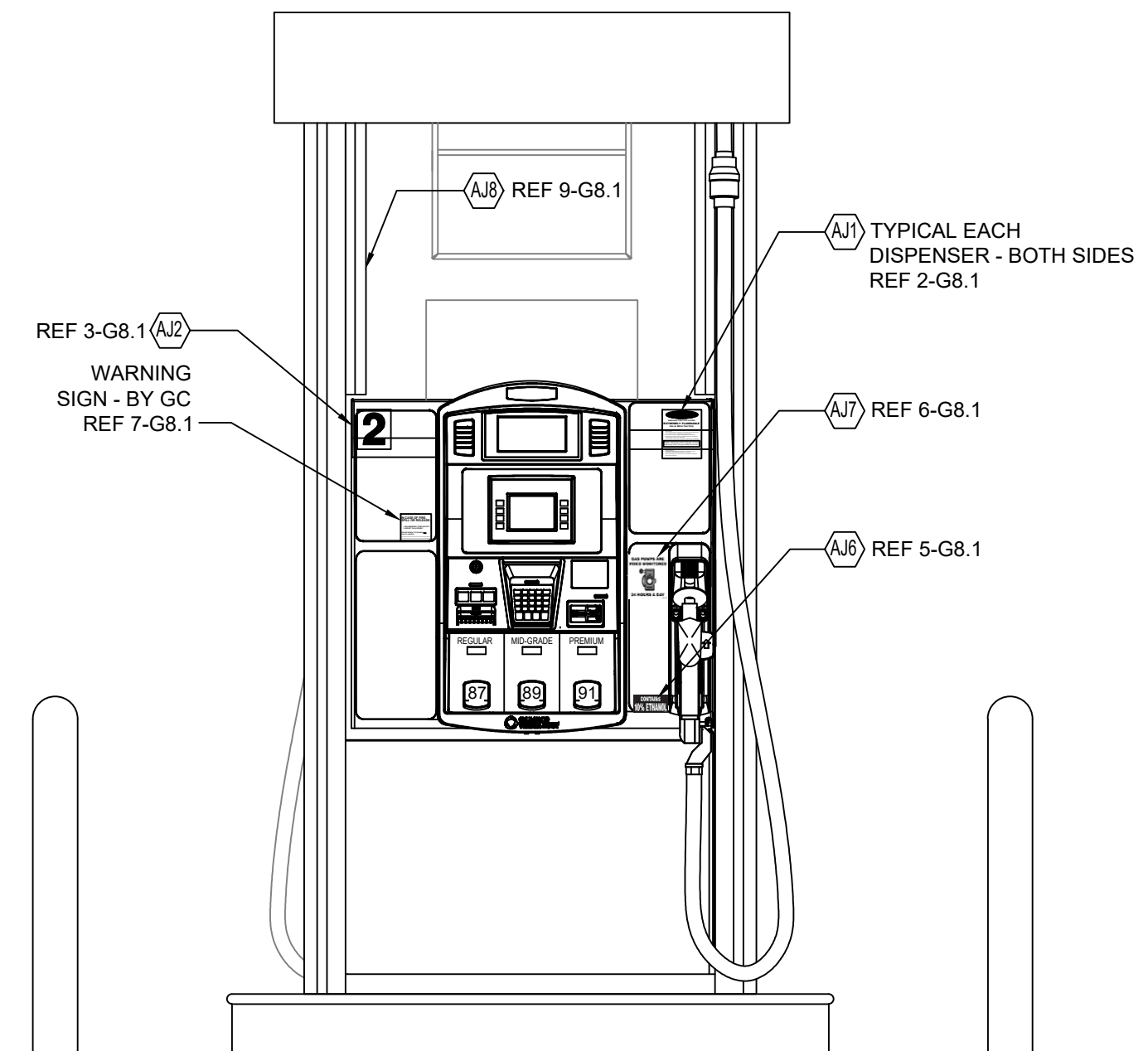
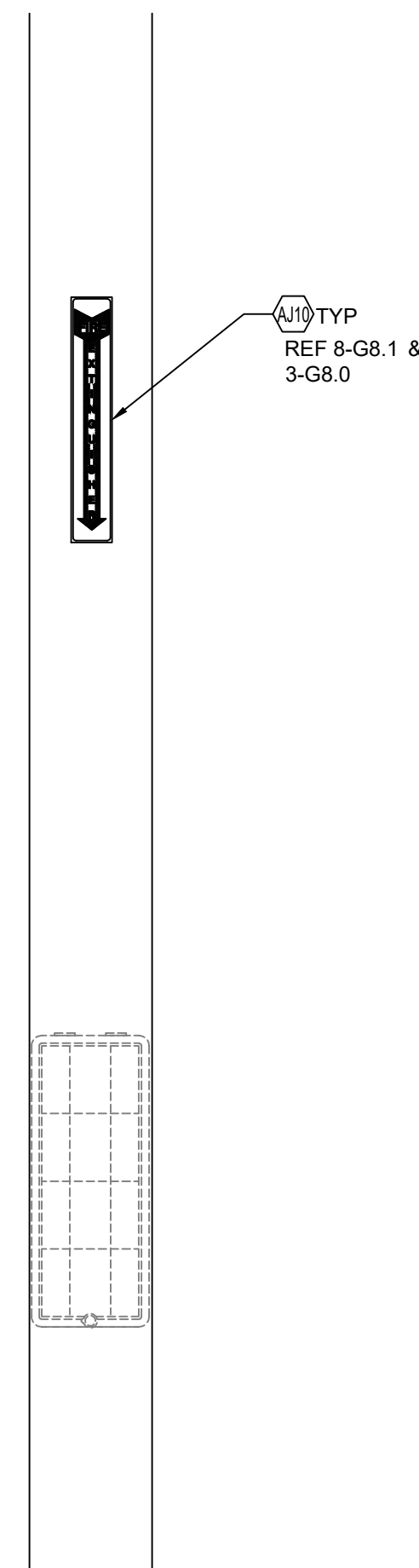
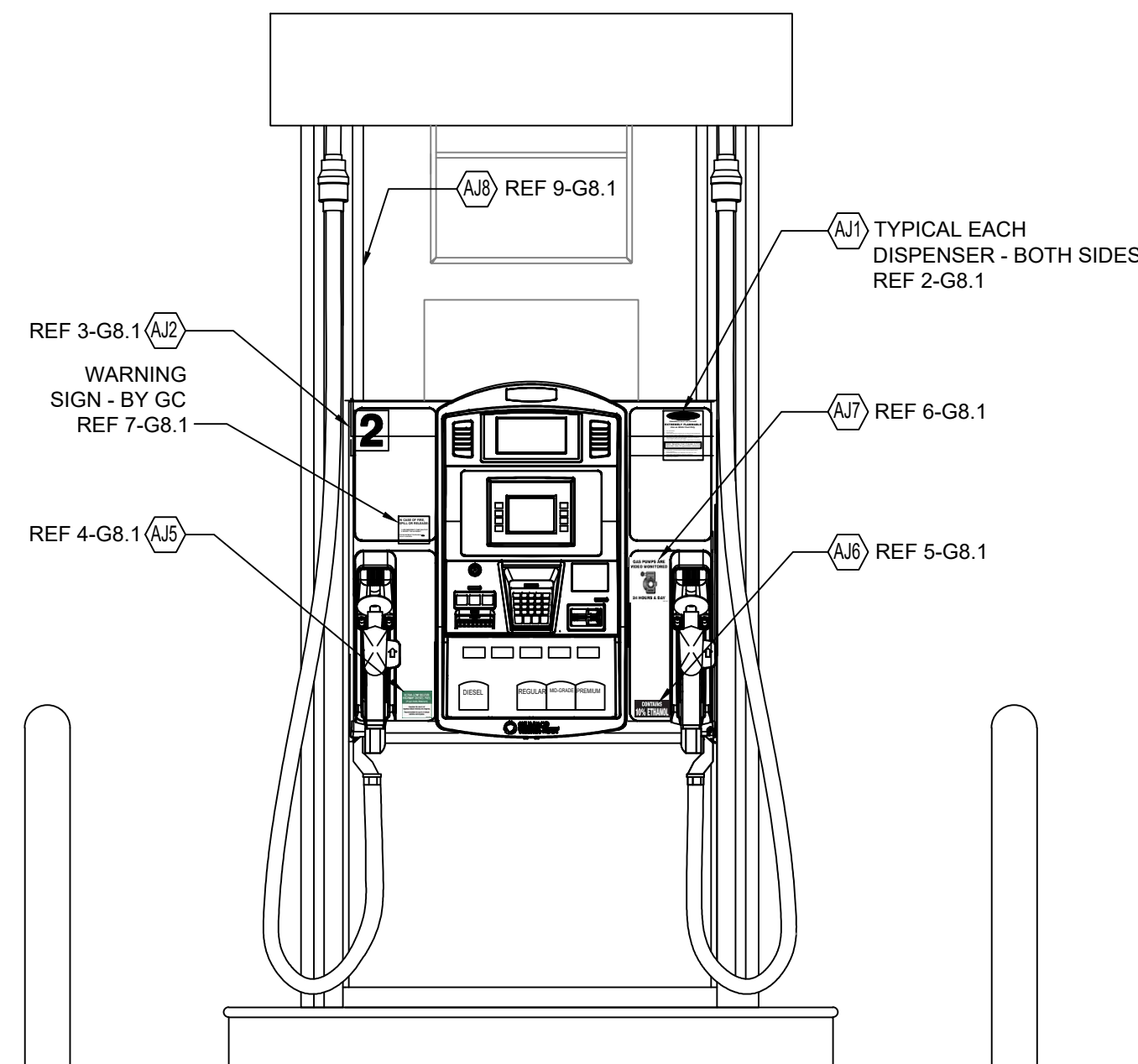
5 CONTAINS 10% ETHANOL N.T.S.

4 LOW SULFUR HIGHWAY DIESEL FUEL N.T.S.

3 DISPENSER NUMBERS N.T.S.



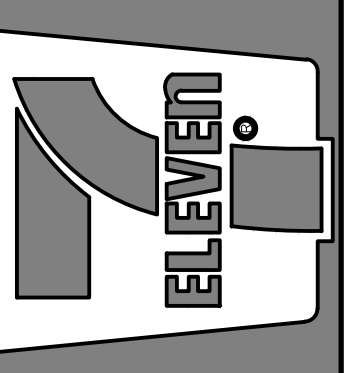
2 EMERGENCY SHUT OFF N.T.S.



1 DISPENSER SIGNAGE ELEVATION 3/4" = 1'-0"

Rev. #	Date	Description

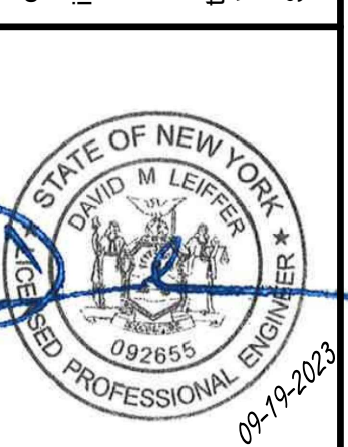
Proto 2022-04
 7-ELEVEN, INC.
 3200 HACKBERRY ROAD, IRVING TEXAS 75063
 7-ELEVEN #46895 - SPEEDWAY #7645
 1542 ROUTE 9
 WAPPINGERS FALLS, NY 12590
 SIGNAGE DETAILS



CORE STATES GROUP
 CORE STATES ARCHITECTURE AND ENGINEERING P.C.
 CERTIFICATE OF AUTHORIZATION NO. 003579
 46 East Main Street
 Suite 201
 NY 10876
 908.46.2370
 core-states.com

Job#: SPW-36100-SEI
 Scale: AS NOTED
 Date: 02/14/23
 Drawn By: RMC
 Checked By: CGC

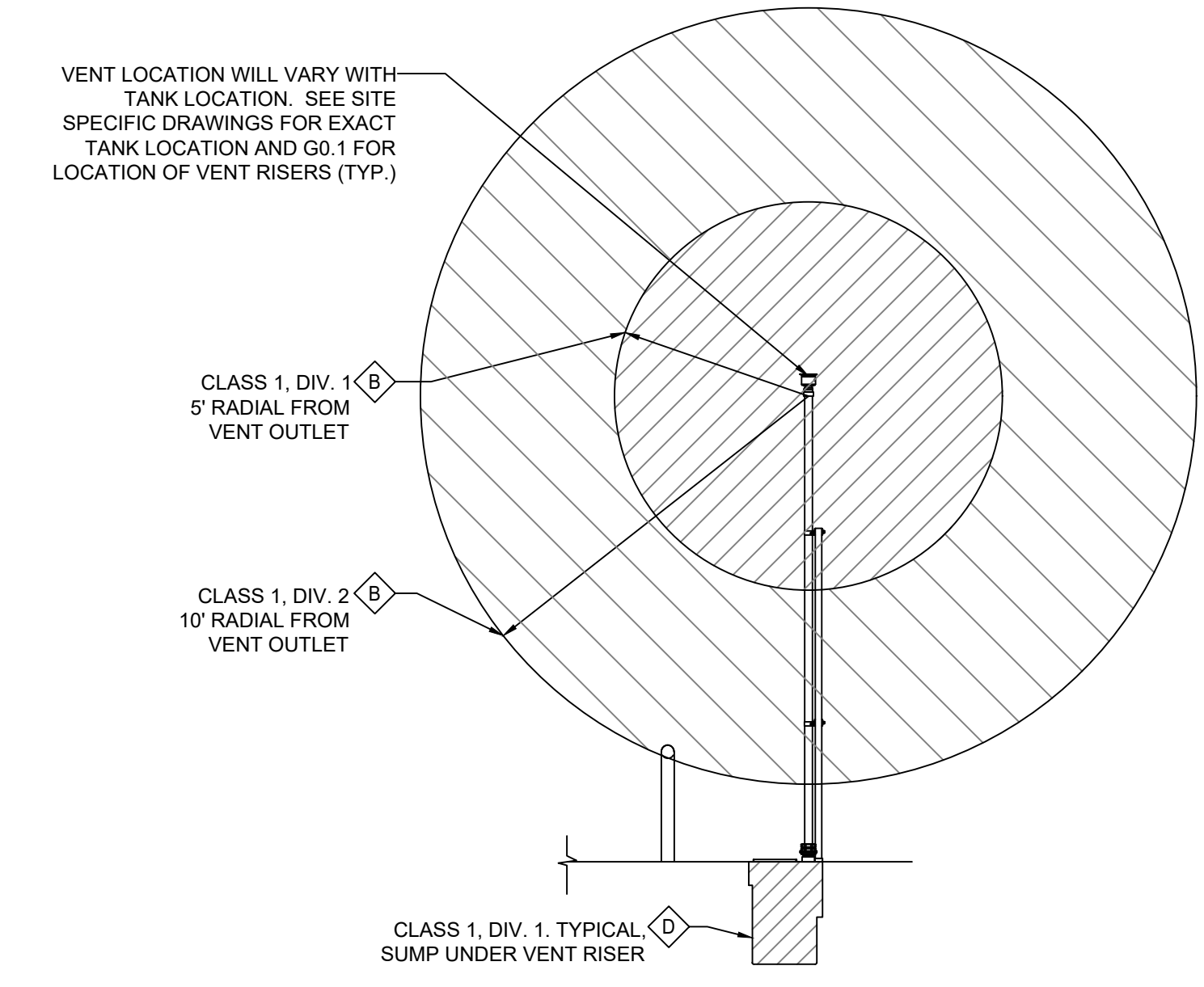
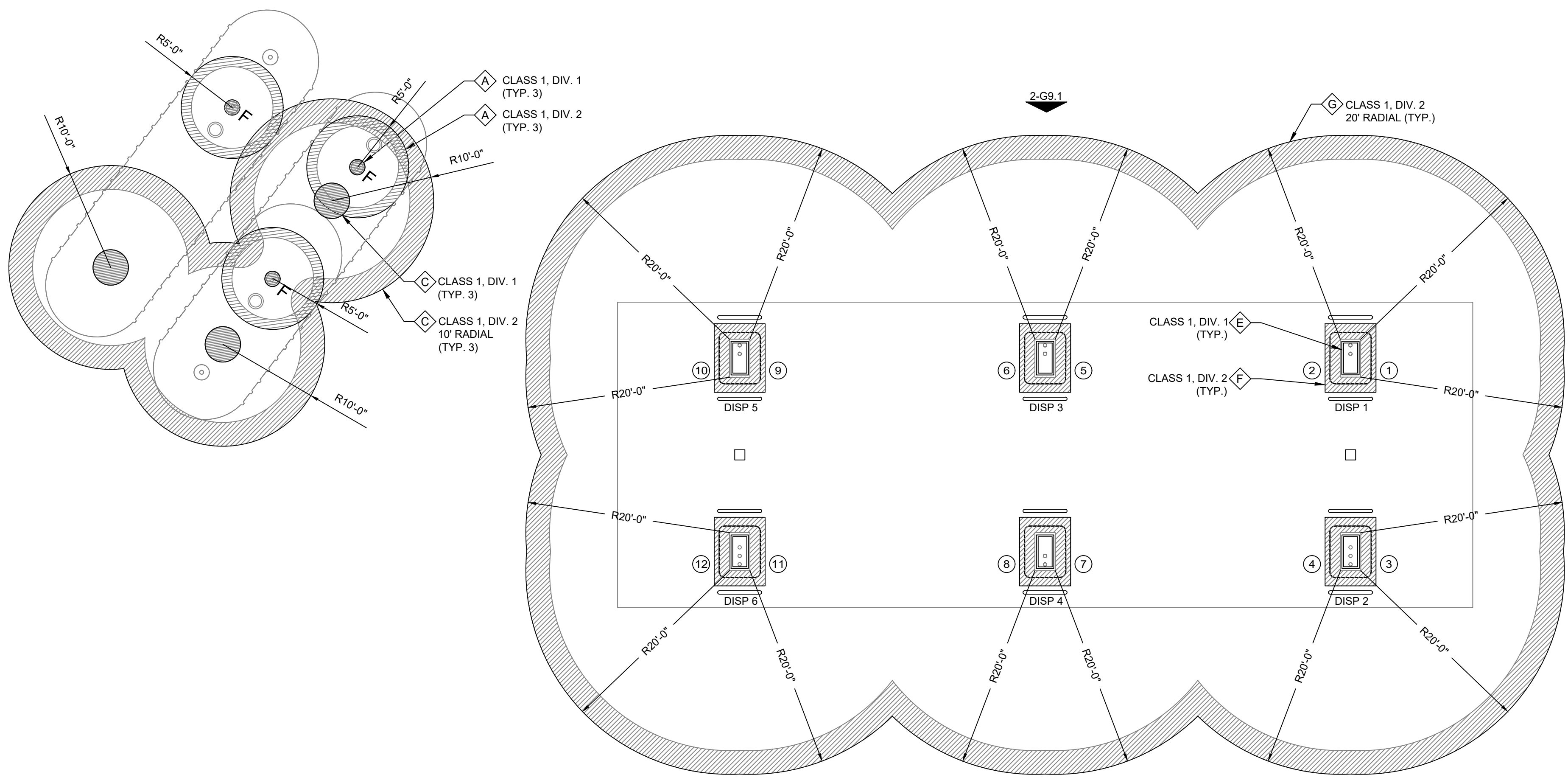
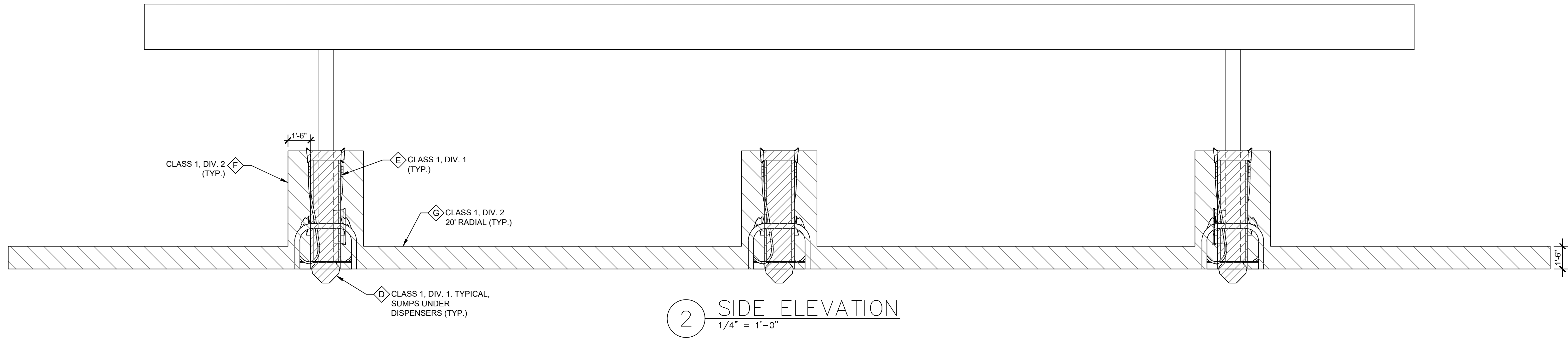
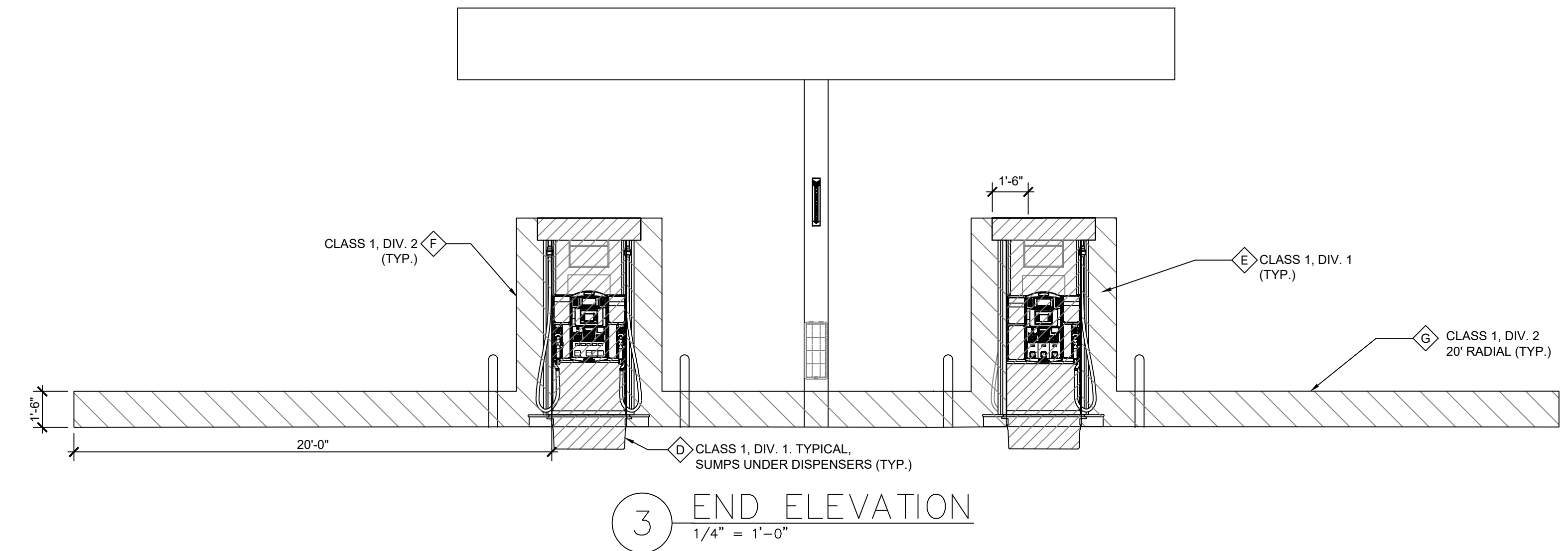
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G8.1
 FUELING - USA

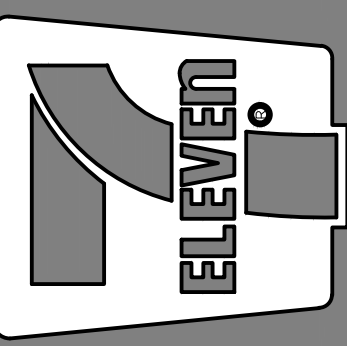
N.E.C. HAZARDOUS AREA NOTES

- A** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (UNDERGROUND TANK - FILL OPENING)
EXTENT OF CLASS 1, GROUP D, DIVISION 1 LOCATION: ANY PIT, BOX, OR SPACE BELOW GRADE LEVEL, ANY PART OF WHICH IS WITHIN THE DIVISION 1 OR 2 CLASSIFIED LOCATION.
- B** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (UNDERGROUND TANK - VENT DISCHARGING UPWARD)
EXTENT OF CLASS 1, GROUP D, DIVISION 1 LOCATION: WITHIN 5 FEET OF OPEN END OF VENT, EXTENDING IN ALL DIRECTIONS.
- C** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (REMOTE PUMP - OUTDOOR)
EXTENT OF CLASS 1, GROUP D, DIVISION 1 LOCATION: ANY PIT, BOX, OR SPACE BELOW GRADE LEVEL IF ANY PART IS WITHIN A HORIZONTAL DISTANCE OF 10 FEET FROM ANY EDGE OF PUMP.
- D** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (DISPENSING DEVICE AND REMOTE VENT - PITS)
EXTENT OF CLASS 1, GROUP D, DIVISION 1 LOCATION: ANY PIT, BOX, OR SPACE BELOW GRADE LEVEL, ANY PART OF WHICH IS WITHIN THE DIVISION 1 OR 2 CLASSIFIED LOCATION.
- E** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (DISPENSING DEVICE - DISPENSER)
EXTENT OF CLASS 1, GROUP D, DIVISION 1 LOCATION: SPACE CLASSIFICATION INSIDE THE DISPENSER ENCLOSURE IS COVERED IN ANSIIUL 87, "POWER OPERATED DISPENSING DEVICES FOR PETROLEUM PRODUCTS."
- F** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (DISPENSING DEVICE - DISPENSER)
EXTENT OF CLASS 1, GROUP D, DIVISION 2 LOCATION: WITHIN 18 INCHES HORIZONTALLY IN ALL DIRECTIONS EXTENDING TO GRADE FROM (1) THE DISPENSER ENCLOSURE OR (2) THAT PORTION OF THE DISPENSER ENCLOSURE CONTAINING LIQUID HANDLING COMPONENTS. SPACE CLASSIFICATION INSIDE THE DISPENSER ENCLOSURE IS COVERED IN ANSIIUL 87, "POWER OPERATED DISPENSING DEVICES FOR PETROLEUM PRODUCTS."
- G** TYPICAL N.E.C. ARTICLE 514 CLASS 1 LOCATION (DISPENSING DEVICE - OUTDOOR)
EXTENT OF CLASS 1, GROUP D, DIVISION 2 LOCATION: UP TO 18 INCHES ABOVE GRADE LEVEL WITHIN 20 FEET HORIZONTALLY OF ANY EDGE OF ENCLOSURE.



Rev. #	Date	Description

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
N.E.C. CLASSIFIED AREA



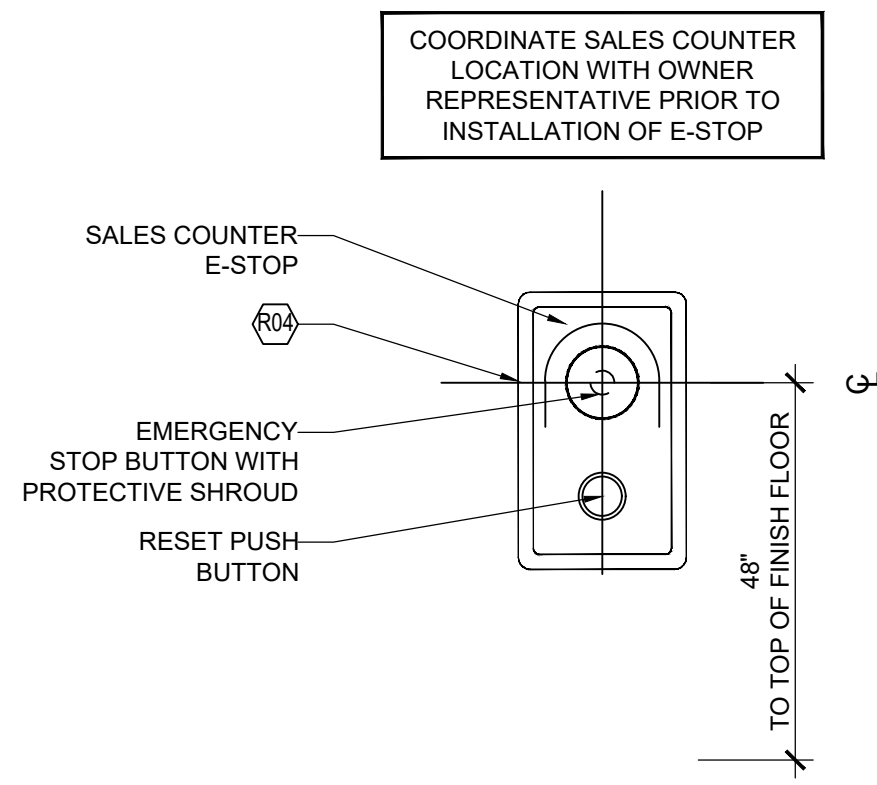
CORE STATES GROUP
CONCRETE ARCHITECTURE AND ENGINEERING P.C.
46 East Main Street
Suite 200A, NJ 08876
908.462.9700
core-states.com
NO. 007879

Job#: SPW-36100-SEI
Scale: AS NOTED
Date: 02/14/23
Drawn By: RMC
Checked By: CGC

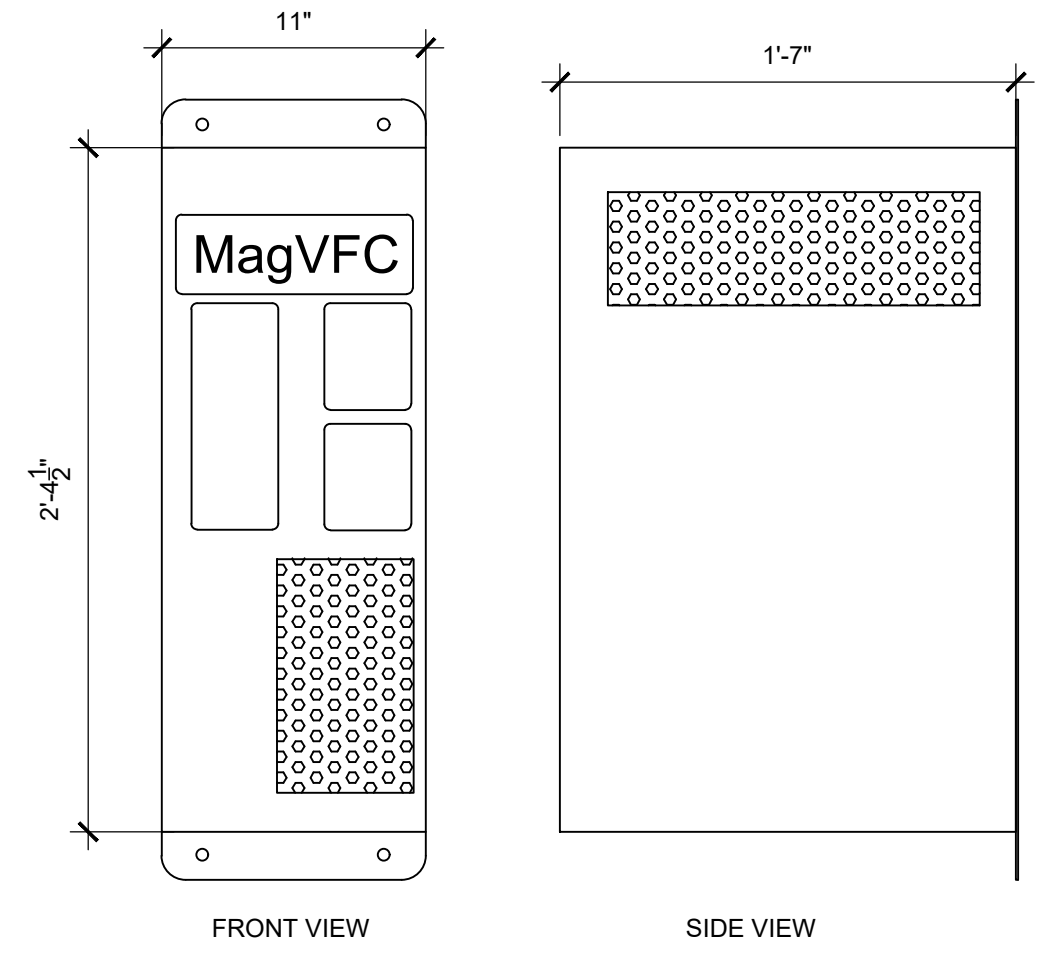
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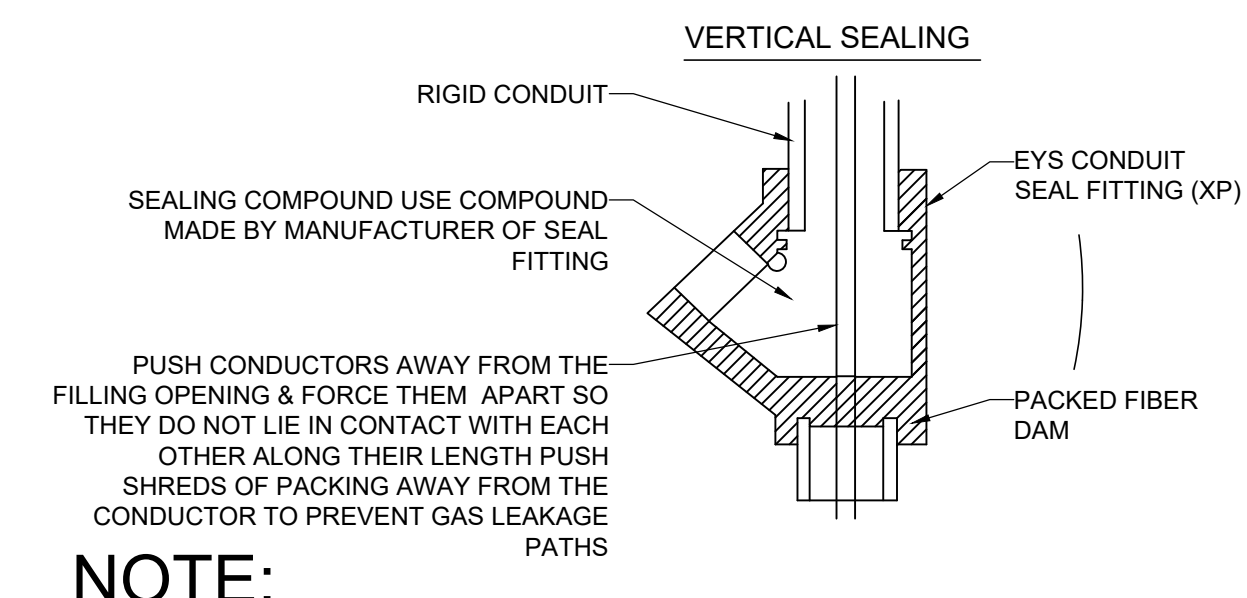
SHEET: **G9.1**
FUELING - USA



5 INTERIOR E-STOP DETAIL
NTS

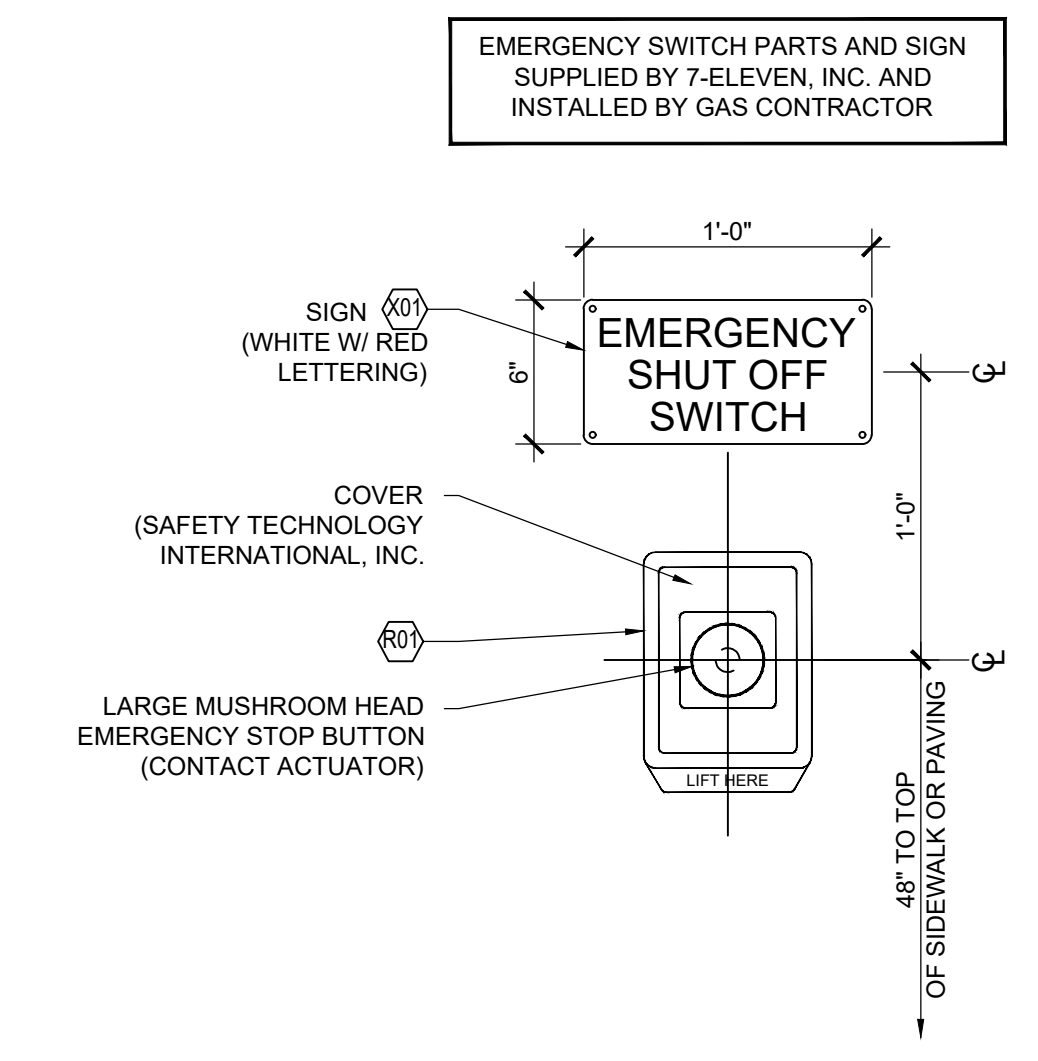


4 MAG VFC DETAIL
NTS

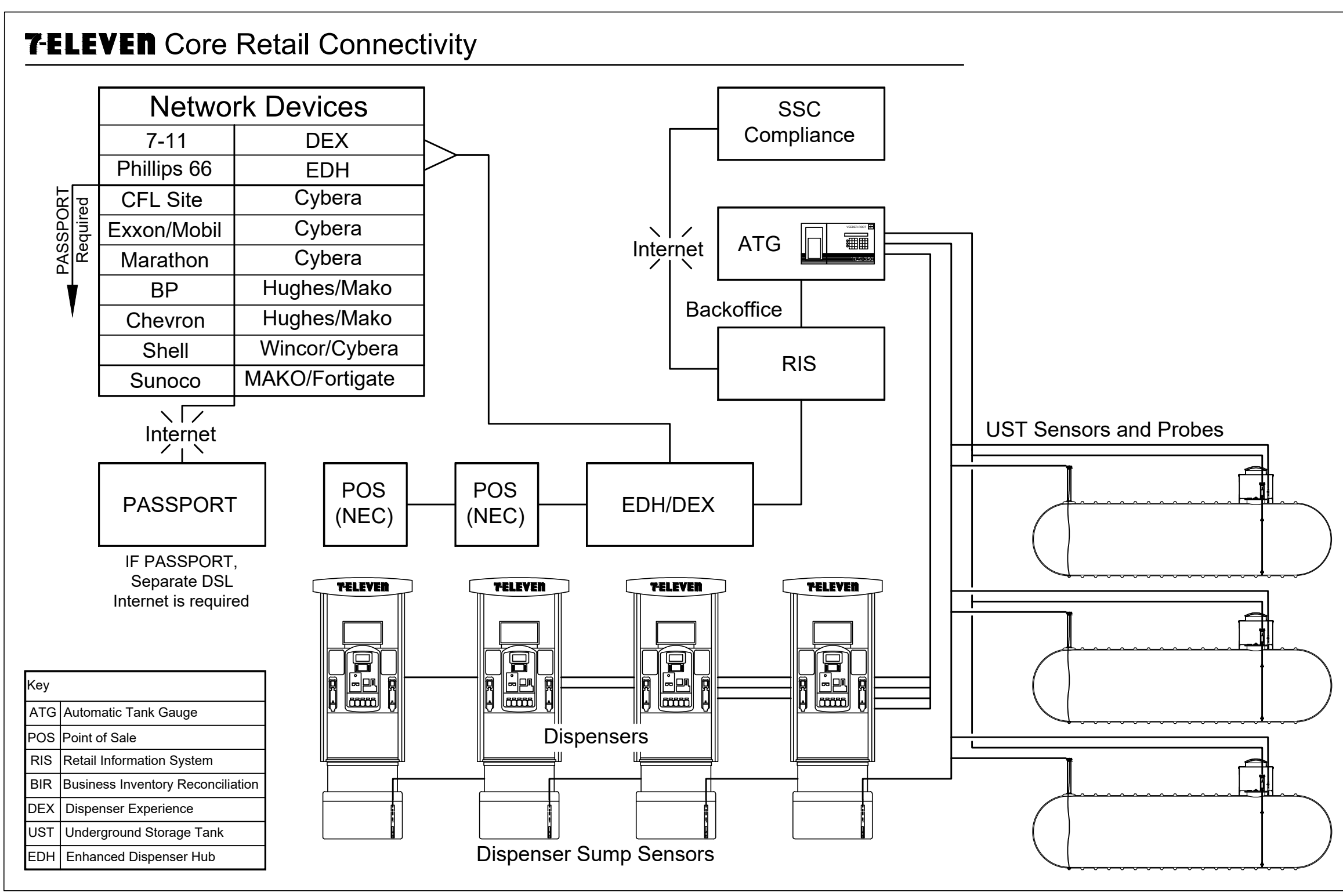


NOTE:
PROVIDE EXPLOSION PROOF SEALS. IN ACCORDANCE WITH ARTICLE 501, CLASS 1, DIVISION 2 OF THE LATEST NATIONAL ELECTRICAL CODE TYPICAL FOR ALL UNDERGROUND CONDUIT STUB-UPS INTO EQUIPMENT.
SEAL OFF ALL CONDUIT ENTERING OR LEAVING CAVITIES OR ENCLOSURES OF DISPENSERS AND ASSOCIATED EQUIPMENT. IN ADDITION, ALL CONDUITS COMING FROM THE BUILDING THROUGH A HAZARDOUS LOCATION ALSO REQUIRE SEAL-OFFS AT BOTH ENDS.

3 SEAL FITTING INSTALLATION
NTS



2 EMERGENCY SHUT OFF W/ COVER
NTS



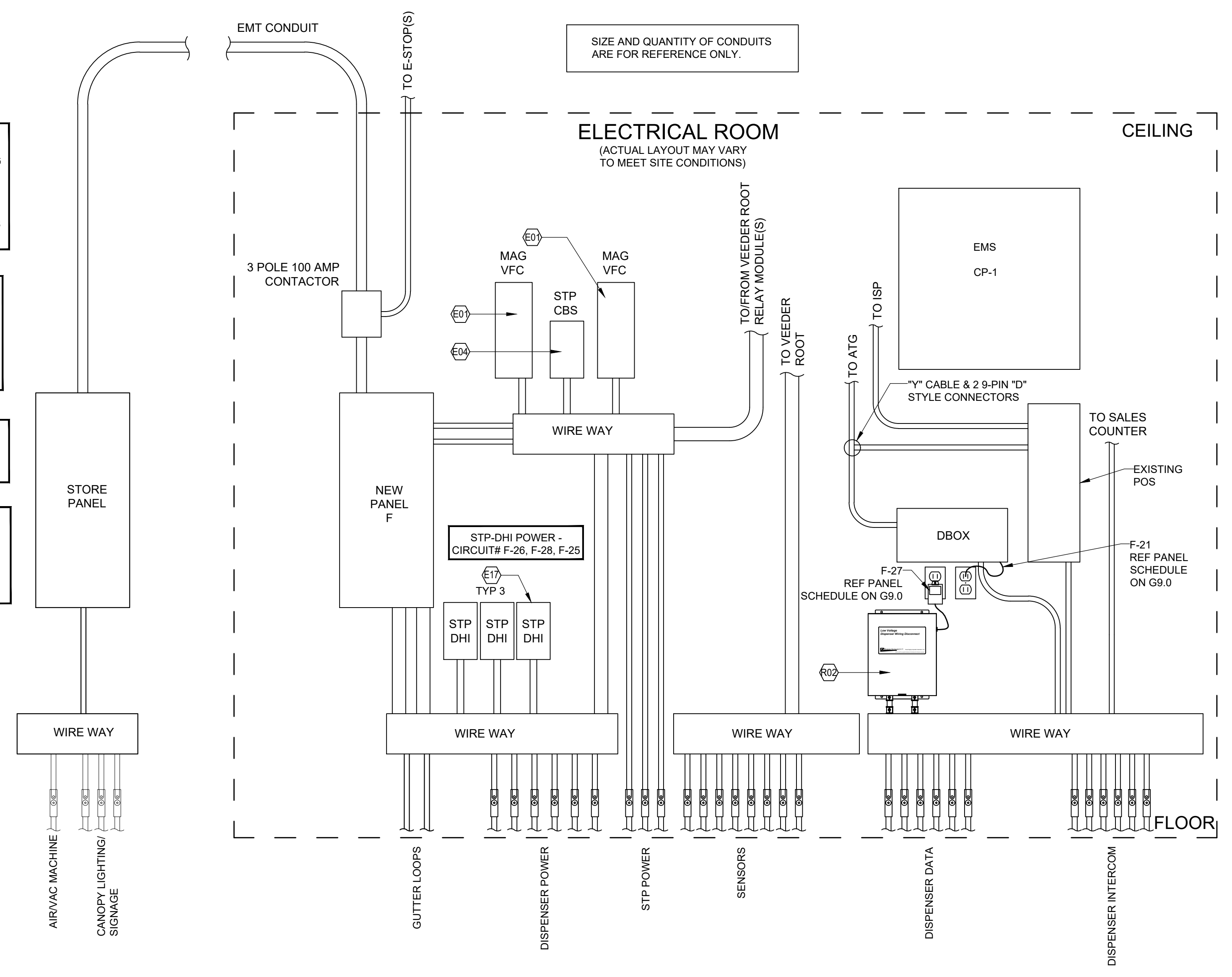
6 CORE RETAIL CONNECTIVITY DIAGRAM
NTS

FUELING CONTRACTOR:
EXTEND CONDUITS AS REQUIRED TO NEW DISPENSERS AND FUEL TANKS. TIE TO EXISTING WHERE IT IS POSSIBLE. SEE SPECIFICATION SHEET G16.0, SECTION VII, 2.3.1 FOR CONDUIT TYPES.
NOTE: PULL ALL NEW WIRES AND PULL STRINGS AS REQUIRED.

NOTE: REARRANGE LAYOUT OF EQUIPMENT AS REQUIRED TO FIT NEW PANEL, ISOLATION RELAY BOXES, CONTROL BOXES, NEW CONDUITS, DBOX AND LOW-VOLTAGE DISCONNECT IN EXISTING ELECTRICAL ROOM. REMOVE OLD UNUSED PANELS AND BOXES AS REQUIRED TO MAKE ROOM FOR NEW EQUIPMENT.

NOTE: TIE NEW DISPENSER AND SUMP CONDUIT INTO EXISTING ELECTRICAL ROOM AS REQUIRED. FIELD VERIFY EXISTING CONDITIONS.

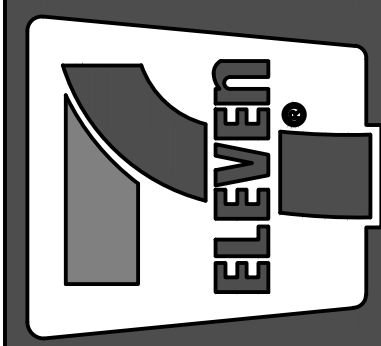
NOTE: REFERENCE SHEET G11.0 FOR DISPENSER HOOK ISOLATION WIRING DIAGRAMS, STP CONTROLLER, AND VEEDER ROOT RELAY MODULE WIRING DIAGRAMS.



1 POWER/DATA EQUIPMENT DETAILS
NTS

Rev. #	Date	Description
Proto 2022-04		

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7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
MISCELLANEOUS ELECTRICAL
DETAILS

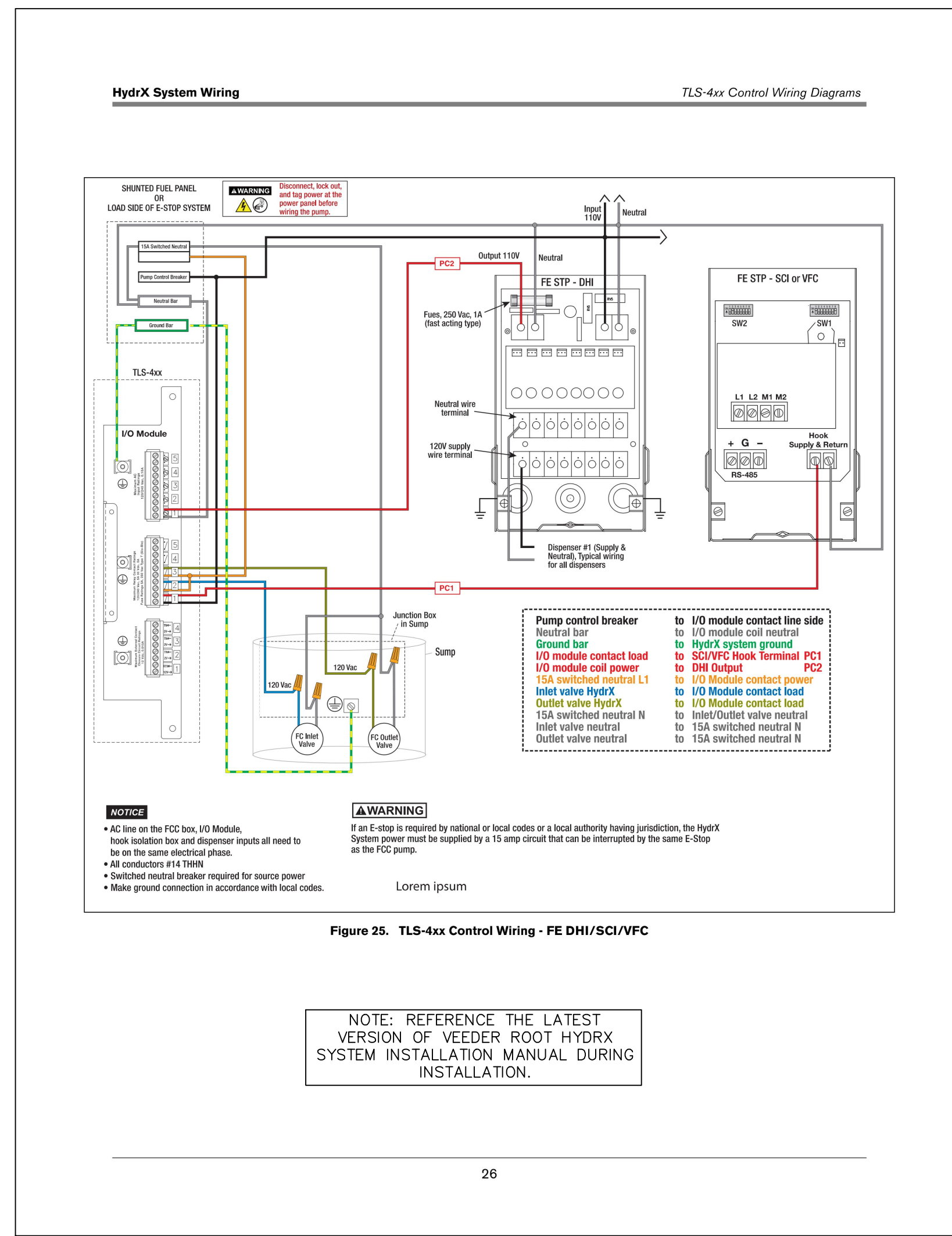


CORE STATES GROUP
CORE STATES ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION NO. 020379
46 East Main Street
Suite 200A, NJ 08876
908.462.9700
core-states.com

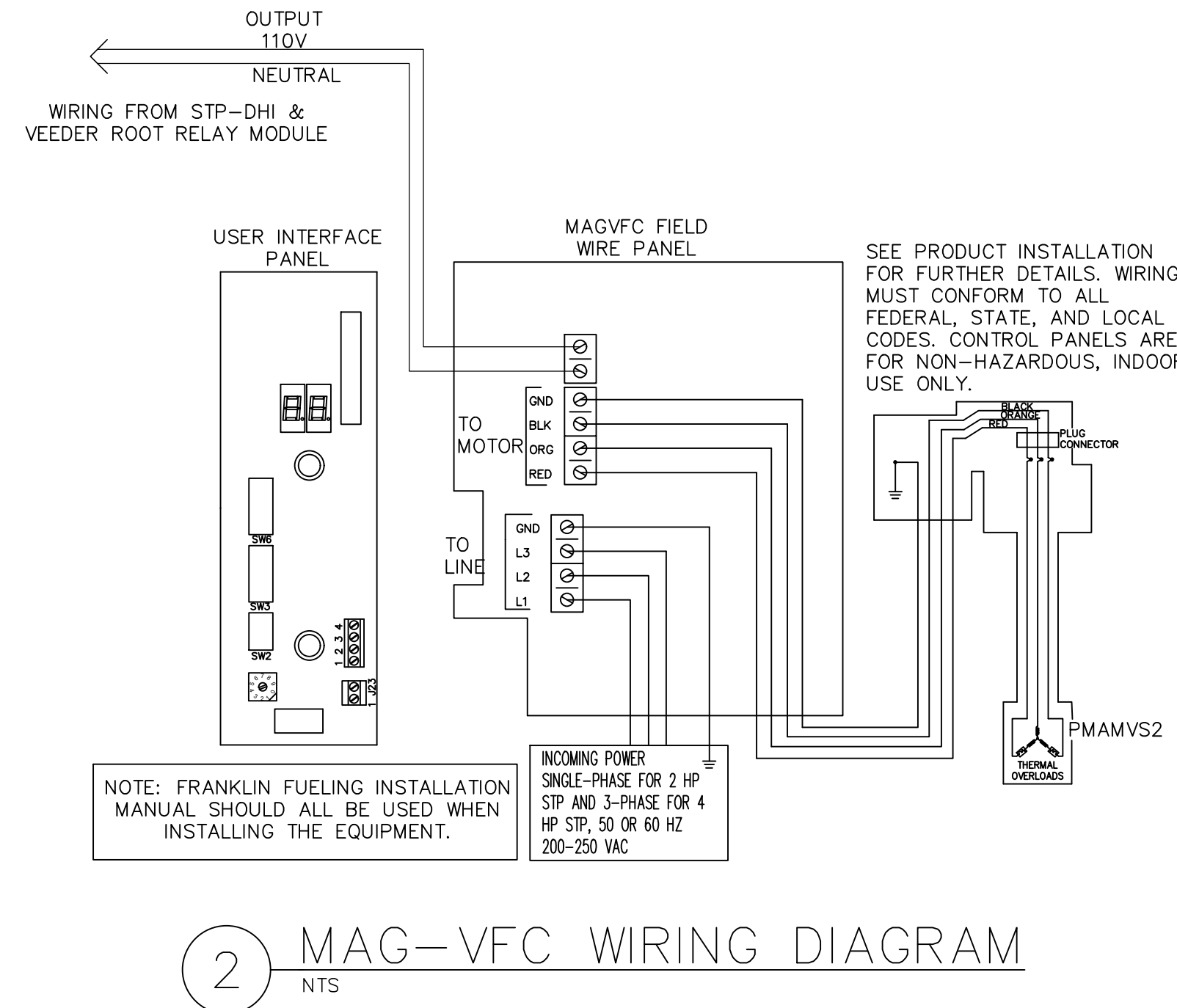
Job#: SPW-36100-SEI	Scale: AS NOTED	Date: 02/14/23	Drawn By: RMC	Checked By: CGC
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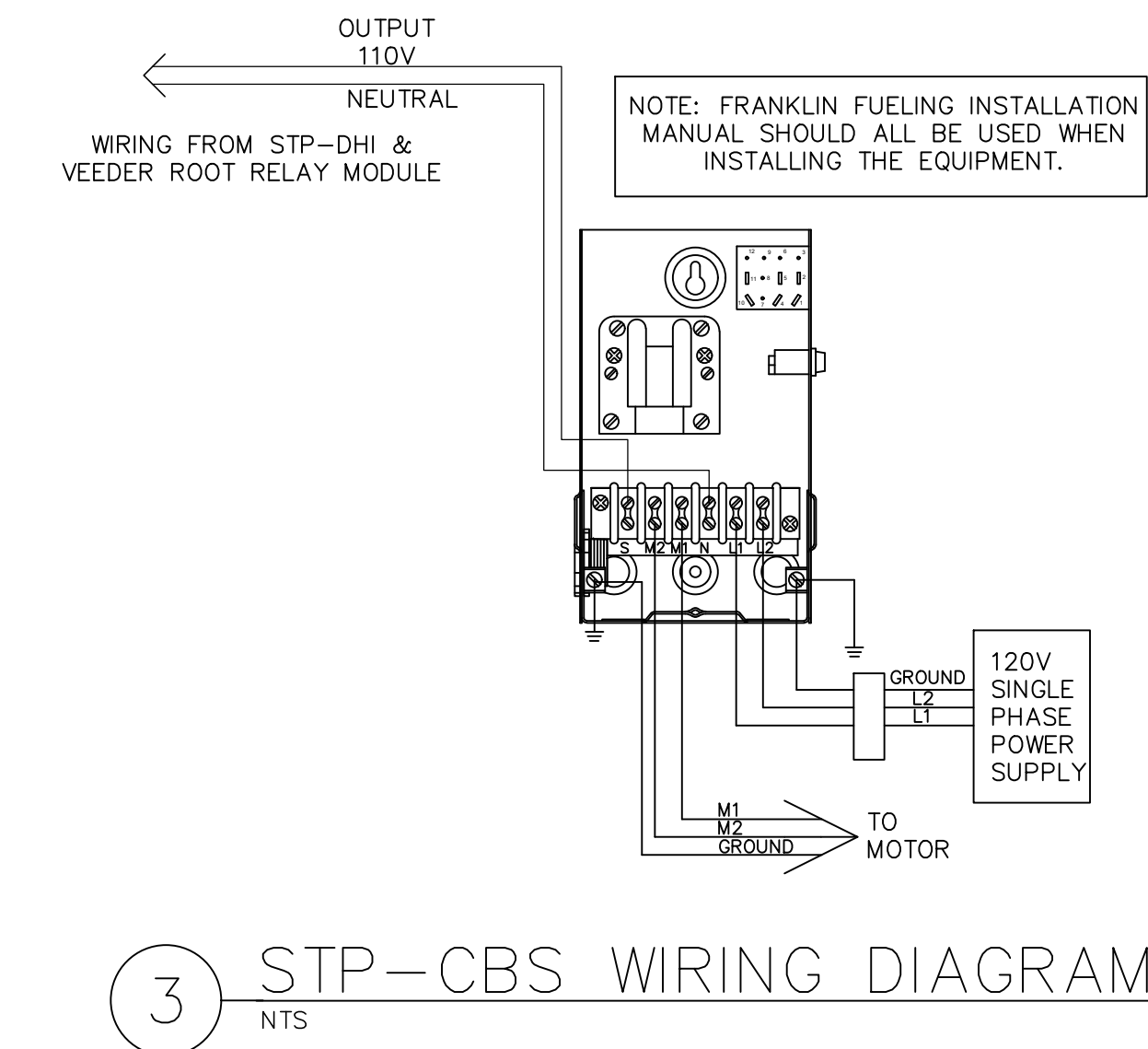




4 HYDRX SYSTEM WIRING DIAGRAM
NTS



2 MAG-VFC WIRING DIAGRAM
NTS

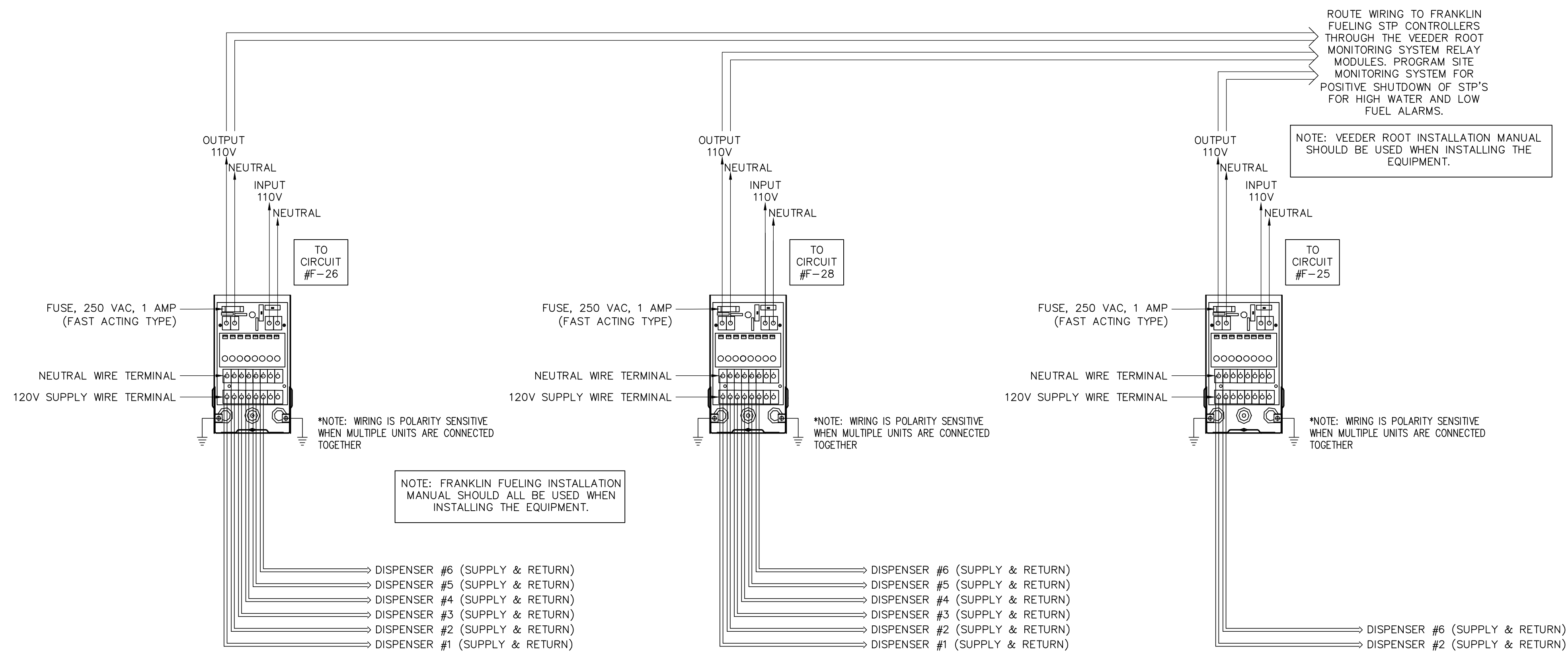


- GENERAL NOTES**
- INSTALL POWER BREAKERS TO EACH CIRCUIT LEADING TO THE DISPENSING UNIT AND STP. THEY MUST BE CAPABLE OF SIMULTANEOUSLY DISCONNECTING HOT AND NEUTRAL CONDUCTORS.
 - INSTALL CONDUIT PER NEC FOR HAZARDOUS LOCATIONS.
 - WIRES - ALL WIRES ARE 14AWG (STANDARD) UNLESS OTHERWISE NOTED. PUMP / DISP. GROUND - WIRE IS 12 AWG (STANDARD). POWER LOADING AND DISTANCE RUN MAY REQUIRE LARGER WIRE SIZE.
 - INSTALL A SINGLE EMERGENCY POWER CUTOFF CONTROL TO REMOVE AC POWER FROM SITE DISPENSING EQUIPMENT. (THE CONTROL IS AN ADDITION SAFETY FEATURE AND NOT A SUBSTITUTE FOR NEC/FPA30 CIRCUIT BREAKER REQUIREMENTS).
 - * LABEL THE EMERGENCY POWER CUTOFF SWITCH AND INSTRUCT OWNER TO KEEP AREA CLEAR OF OBSTACLES.
 - CONNECT INSULATED GROUNDING CONDUCTOR FROM THE DISPENSER POWER PANEL TO THE SITE GROUNDING ELECTRODE. SIZE PER NEC.
 - REFER TO INSTALLATION ADDENDA FOR SPECIFIC PRODUCT FIELD WIRING CONNECTIONS. CAP ALL UNUSED WIRES. LOCAL AND NEC MAY APPLY.
 - STP ISOLATION RELAY BOXES ARE MANDATED BY THE NEC (ARTICLE 514.13 - PROVISIONS FOR MAINTENANCE AND SERVICE OF DISPENSING EQUIPMENT) AND ARE REQUIRED TO:
 - A. ALLOW SERVICE OF ONE UNIT SAFELY WITHOUT REMOVING POWER FROM ALL DISPENSING EQUIPMENT.
 - B. PREVENT DAMAGE TO EQUIPMENT FROM CROSS-PHASING.
 DAMAGE CAUSED BY CROSS-PHASING IS NOT COVERED BY WARRANTY.

REGULAR UNLEADED DISPENSER HOOK ISOLATION

PREMIUM UNLEADED DISPENSER HOOK ISOLATION

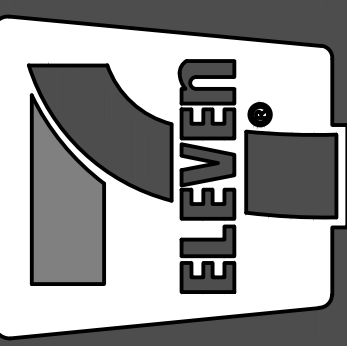
DIESEL DISPENSER HOOK ISOLATION



1 DISPENSER HOOK ISOLATION WIRING DIAGRAM
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7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
ISOLATION RELAY BOX DETAIL



CORE STATES GROUP

CONCRETE ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION NO. 001079

46 East Main Street
Suite 200A, NJ 08876
908.462.9700
core-states.com

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NON-HAZARDOUS AREA

HAZARDOUS AREA

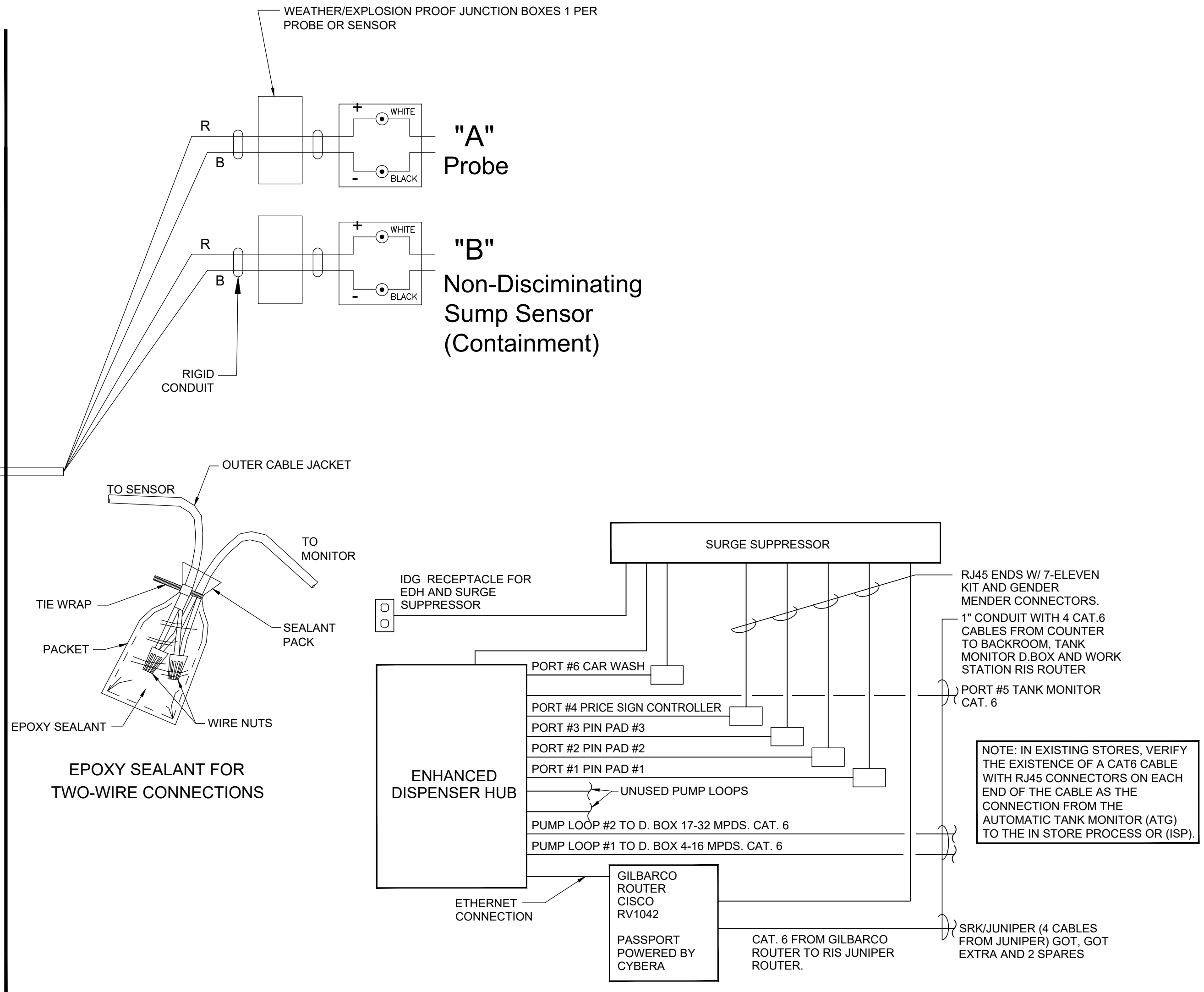
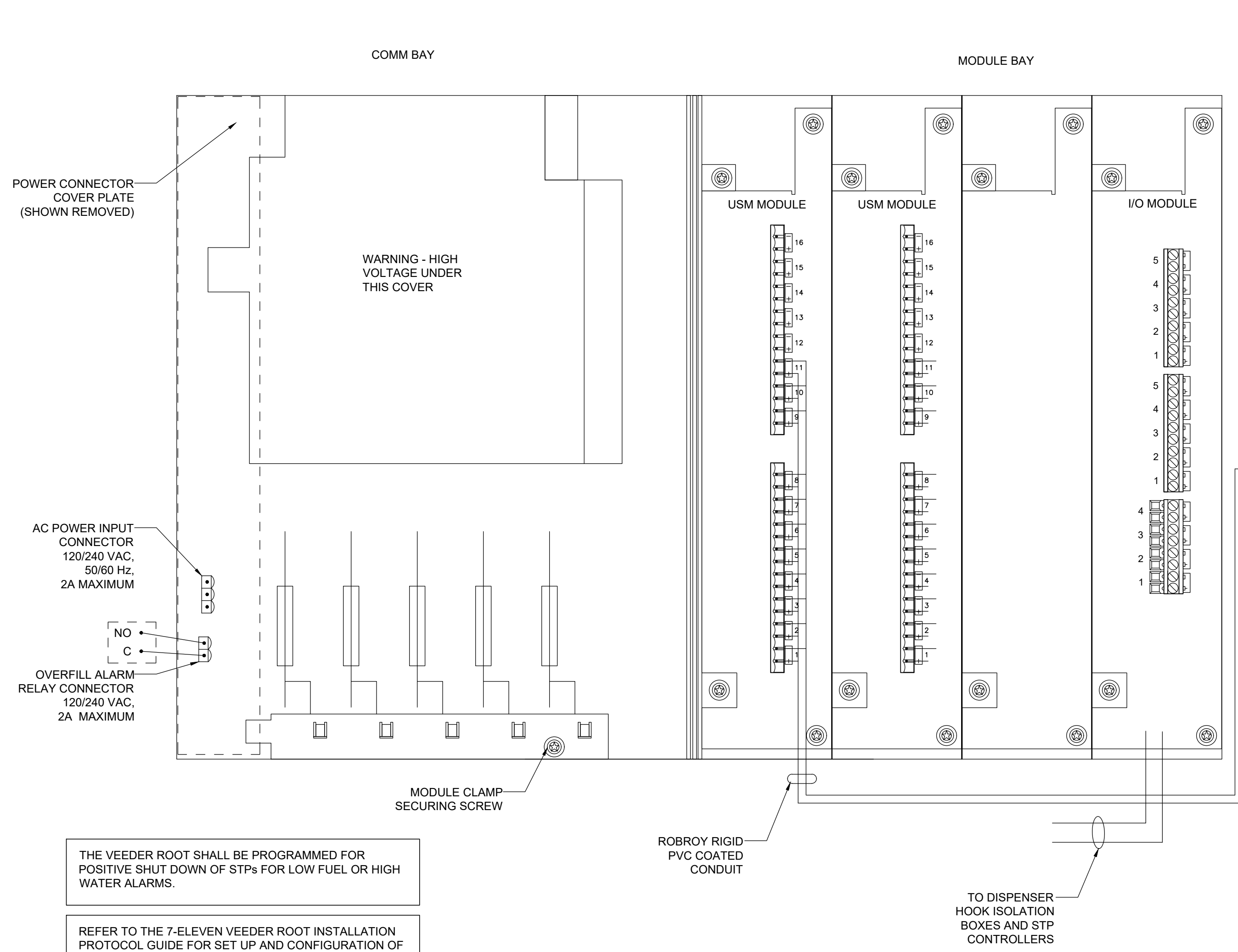
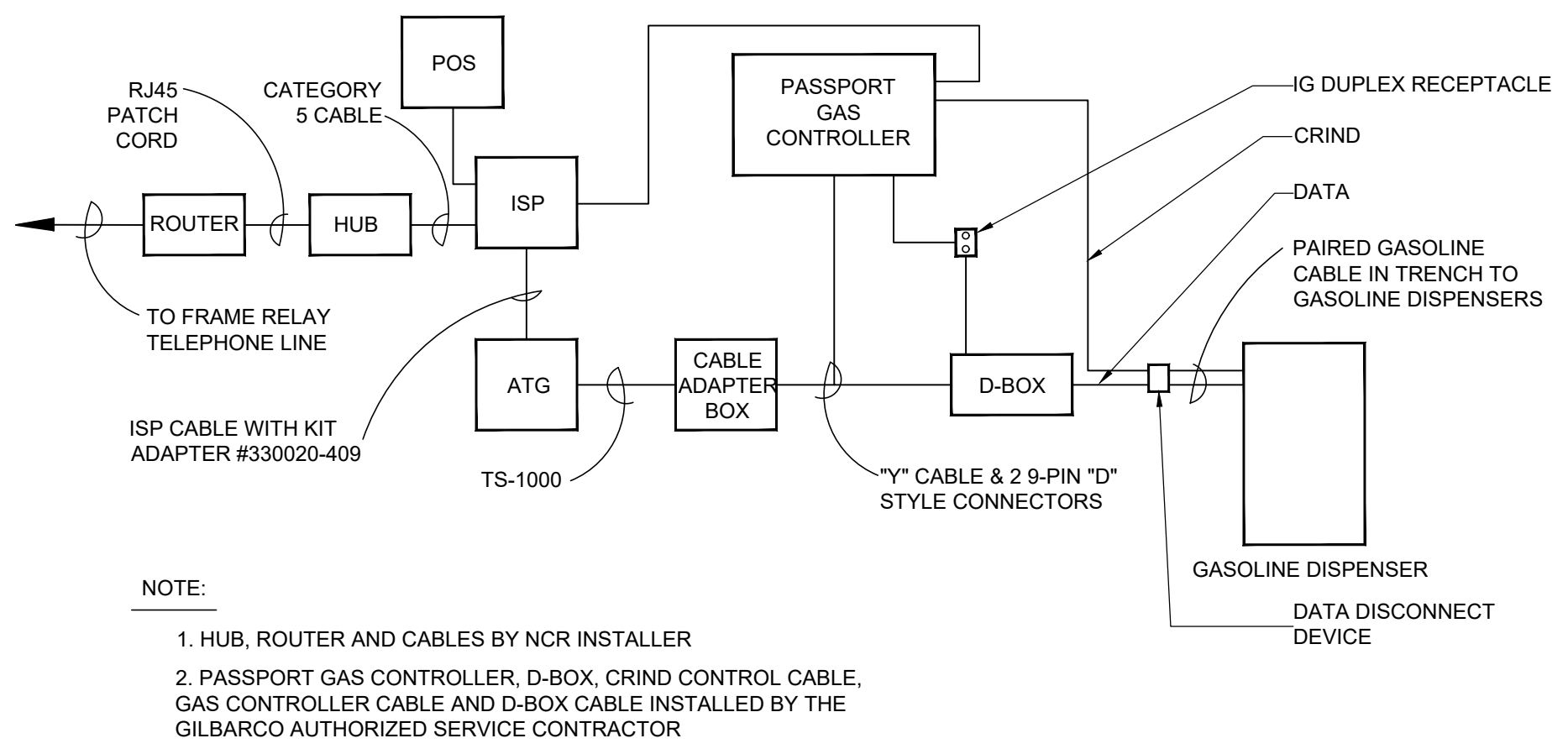


TABLE 1 • WIRE TYPES AND SIZES

CABLE TYPE	PROBE OR SENSOR
BELDEN 88760	PROBE, SUMP, LIQUID, AND VACUUM SENSOR
12 AWG THWN	AC POWER TO CONSOLE

1 SITE MONITORING WIRING SCHEMATIC
1 1/2" = 1'-0"

3 ENHANCED DISPENSER HUB
NTS



2 POS GAS CONTROLLER DIAGRAM
NTS

GENERAL NOTES

IMPORTANT: THIS IS A CONTROL DRAWING ONLY AND DOES NOT REFLECT THE ACTUAL LOCATIONS OF CONDUIT ENTRY (SEE NOTE 7 BELOW).

VEEDER-ROOT REQUIRES THAT ANYONE INSTALLING OUR EQUIPMENT MUST BE LEVEL I CERTIFIED. FOR MORE INFORMATION REGARDING THE HOME STUDY INSTALLERS COURSE CONTACT JOHN HOWELL 951-532-8599.

WARNING: IN INSTALLATION AND USE OF THIS PRODUCT, COMPLY WITH THE NATIONAL ELECTRICAL CODE, FEDERAL, STATE AND LOCAL CODES. IN ADDITION, TURN OFF POWER AND TAKE OTHER NECESSARY PRECAUTIONS DURING INSTALLATION, SERVICE AND REPAIR TO PREVENT PERSONAL INJURY, PROPERTY LOSS AND EQUIPMENT DAMAGE.

WARNING: DISCONNECT ALL POWER BEFORE MAKING ANY CONNECTIONS TO PREVENT DEATH, SERIOUS INJURY, EXPLOSION, OR ELECTRICAL SHOCK. MONITOR MUST NEVER BE OPERATED UNLESS THE FRONT COVER IS CLOSED OVER THE BARRIER TERMINALS IN THE INTRINSICALLY SAFE AREA.

1. INTRINSICALLY SAFE WIRING
BONDED CONDUIT MEANS THAT THE METALLIC SECTIONS OF CONDUIT ARE PERMANENTLY JOINED TO FORM AN ELECTRICALLY CONDUCTIVE PATH THAT WILL ASSURE ELECTRICAL CONTINUITY, AND THAT THE CONDUIT HAS THE CAPACITY TO CONDUCT SAFELY, ANY CURRENT LIKELY TO BE IMPOSED.

a) BONDED & NON-BONDED CONDUIT - THE WIRES BETWEEN THE MONITOR AND EACH PROBE/SENSOR LOCATION MUST BE #14, #16 OR #18 AWG STRANDED COPPER WIRES WITHIN A SHIELDED CABLE. SHIELDED CABLE MUST BE RATED AT LESS THAN 100 PICOFARAD PER FOOT AND MUST BE MANUFACTURED WITH A MATERIAL SUITABLE FOR THE ENVIRONMENT, SUCH AS BELDEN™ 88760.

2. CONNECT THE BARRIER GROUND TO THE EARTH GROUND BUS AT THE POWER DISTRIBUTION PANEL WITH #12 AWG (OR LARGER) CONDUCTOR.

3. DENOTES FIELD WIRING CONNECTION USING WATERPROOF CONNECTORS SUPPLIED WITH THE PROBE(S) AND SENSOR(S).

4. INTRINSICALLY SAFE WIRING SHALL BE INSTALLED IN ACCORDANCE WITH ARTICLE 504-20 OF THE NEC, ANSINFPFA 70.

5. TO MAINTAIN INTRINSIC SAFETY, PROBE/SENSOR WIRING MUST BE INSTALLED WITH EITHER SEALED CONDUIT OR DIRECT BURIAL METHODS. REFER TO "DIRECT BURIAL CABLE INSTALLATION", MANUAL NO. 576013-858.

6. ELECTRICAL RATING POWER INPUT 120 VAC, 50/60 HZ, 100 VA MAXIMUM.

7. THIS DOCUMENT IS NOT TO BE USED AS A SUBSTITUTE FOR SPECIFIC EQUIPMENT INSTALLATION MANUALS. FOR INSTALLATION DETAILS USE THE RESPECTIVE MANUAL. TLS-450 PLUS 577014-073

8. ALL CABINET PENETRATIONS MUST ENTER THROUGH A PRE-DRILLED CONDUIT KNOCKOUT.

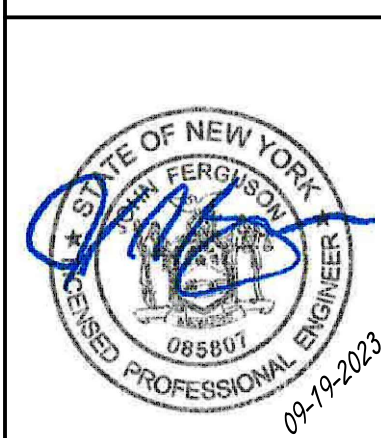
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7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
SITE MONITORING EQUIPMENT DIAGRAM

CORE STATES GROUP
CONCRETE
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 020379
46 East Main Street
Suite 200
Suffolk, VA 23080
908.462.9700
core-states.com

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CONTRACTOR NOTE:
PLEASE COMPLETE THE FOLLOWING ITEMS WHEN REPLACING AN EXISTING ATG WITH A NEW TLS-450 PLUS CONSOLE:

- WRACO MUST BE COMPLETED ONLINE FOR THE TLS-450 PLUS (WARRANTY REGISTRATION AND CHECKOUT). COPY TO BE PROVIDED TO SOURCE.
- SERIAL NUMBER OF EXISTING TLS UNIT, OR COMPETITIVE UNIT, THAT IS BEING REPLACED.
- PHOTO OF UNIT BEING DESTROYED IN THE FIELD.

SUBMIT ALL 3 COMPONENTS TO SOURCE

PERFORMANCE SPECIFICATION

SECTION I GENERAL CONDITIONS

PART 1 - GENERAL

- 1.1 SCOPE OF WORK: THE WORK INCLUDED IN THE CONTRACT FOR THIS PROJECT CONSISTS OF THE FURNISHING OF ALL LABOR, MATERIALS, SERVICES, EQUIPMENT AND APPLIANCES REQUIRED IN CONJUNCTION WITH OR PROPERLY INCIDENTAL TO THE COMPLETE CONSTRUCTION OF A GASOLINE INSTALLATION FOR 7-ELEVEN, INC.

PART 2 - AGREEMENT

- 2.1 CONTRACTOR SHALL NOT START WORK WITHOUT CONTRACT AND APPROVED SEALED PLANS.

PART 3 - INTENT OF DRAWING AND SPECIFICATIONS

- 3.1 THE INTENT OF THE DRAWINGS AND SPECIFICATIONS IS TO PRESCRIBE A COMPLETE WORK OR IMPROVEMENT WHICH THE CONTRACTOR SHALL PERFORM IN FULL COMPLIANCE WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE WORK IN A SATISFACTORY AND ACCEPTABLE MANNER.
- 3.2 THE CONTRACTOR SHALL, UPON DISCOVERY, REPORT ANY DISCREPANCIES IN THE DRAWINGS AND SPECIFICATIONS TO THE OWNER'S CONSTRUCTION MANAGER.
- 3.3 THE DRAWINGS AND SPECIFICATIONS ARE INTENDED TO BE COMPLIMENTARY. ANYTHING MENTIONED IN THE SPECIFICATIONS AND NOT SHOWN ON THE DRAWINGS, OR SHOWN ON THE DRAWINGS AND NOT MENTIONED IN THE SPECIFICATIONS, SHALL BE OF LIKE EFFECT AS IF SHOWN OR MENTIONED IN BOTH. GENERALLY, THE SPECIFICATIONS DESCRIBE WORK WHICH CANNOT BE READILY IDENTIFIED ON THE DRAWINGS AND INDICATED TYPES, QUALITIES AND METHODS OF INSTALLATION OF THE VARIOUS MATERIALS AND EQUIPMENT REQUIRED FOR THE WORK. IT IS NOT INTENDED TO MENTION EVERY ITEM OF WORK IN THE SPECIFICATIONS WHICH CAN BE ADEQUATELY SHOWN ON THE DRAWINGS NOR TO SHOW ON THE DRAWINGS ALL ITEMS OF WORK DESCRIBED OR REQUIRED BY THE SPECIFICATIONS EVEN IF THEY ARE OF SUCH NATURE THAT THEY COULD HAVE BEEN SHOWN THEREON. ALL MATERIALS OR LABOR FOR WORK WHICH IS SHOWN ON THE DRAWINGS OR IS REASONABLY INFERABLE AS BEING NECESSARY TO PRODUCE A FINISHED JOB SHALL BE PROVIDED BY THE CONTRACTOR WHETHER OR NOT THE WORK IS EXPRESSLY COVERED IN THE SPECIFICATIONS.
- 3.4 THE CONTRACTOR, SUBCONTRACTORS AND ALL TRADES SHALL EXAMINE THE SITE AND ASCERTAIN EXISTING CONDITIONS PRIOR TO THE START OF THEIR PORTIONS OF THE WORK. BEFORE ORDERING MATERIALS OR DOING ANY WORK, THE CONTRACTOR SHALL VERIFY AND BE RESPONSIBLE FOR THE CORRECTNESS OF ALL MEASUREMENTS AT THE SITE. ANY DIFFERENCES WHICH MAY BE FOUND BETWEEN ACTUAL DIMENSIONS AND DIMENSIONS INDICATED ON THE DRAWINGS OR SHOP DRAWINGS SHALL BE SUBMITTED TO THE OWNER'S CONSTRUCTION MANAGER FOR CONSIDERATION BEFORE PROCEEDING WITH THE ACTUAL DIMENSIONS AND MEASUREMENTS INDICATED ON THE DRAWINGS UNLESS A SUBSTANTIAL ERROR HAS BEEN MADE. IF SUCH AN ERROR SHOULD OCCUR, IT SHALL BE BROUGHT TO THE ATTENTION OF THE OWNER'S CONSTRUCTION MANAGER AND RESOLVED BEFORE PROCEEDING WITH WORK.
- 3.5 THE ORGANIZATION OF SPECIFICATIONS INTO DIVISIONS, SECTIONS AND PARAGRAPHS, AND THE ARRANGEMENT OF THE DRAWINGS ARE NOT INTENDED TO CONTROL THE CONTRACTOR IN DIVIDING THE WORK AMONG SUBCONTRACTORS OR IN THE ESTABLISHING THE EXTENT OF WORK TO BE PERFORMED BY ANY TRADE.
- 3.6 DRAWINGS AND SPECIFICATIONS: DRAWINGS INDICATE GENERAL DESIGN AND ARRANGEMENTS DRAWINGS ARE DIAGRAMMATIC AND ARE NOT SCALED FOR DIMENSIONS. TAKE ALL DIMENSIONS FROM ARCHITECTURAL PLANS AND EQUIPMENT TO BE FURNISHED. VERIFY DIMENSIONS IN THE FIELD.

PART 4 - RESPONSIBILITY OF THE CONTRACTOR

- 4.1 CONTRACTOR HEREBY DECLARES HE HAS READ ALL SPECIFICATIONS AND EXAMINED THE DRAWINGS AND THAT HE UNDERSTANDS ALL CONDITIONS.
- 4.2 CONTRACTOR HEREBY DECLARES HE HAS VISITED THE SITE AND IS FAMILIAR WITH THE CONDITIONS AFFECTING THE WORK. NO ALLOWANCES SHALL BE MADE SUBSEQUENTLY ON BEHALF OF THE CONTRACTOR FOR ANY ERROR NEGLIGENCE ON HIS PART. IF IN THE PERFORMANCE OF THE CONTRACT, LATENT CONDITIONS AT THE SITE ARE FOUND TO BE MATERIALLY DIFFERENT FROM THOSE INDICATED BY THE DRAWINGS AND SPECIFICATIONS, OR UNKNOWN CONDITIONS UNUSUALLY INHERENT IN WORK OF THIS CHARACTER SHOWN AND SPECIFIED, THE ATTENTION OF THE OWNER'S CONSTRUCTION MANAGER SHALL BE CALLED IMMEDIATELY TO SUCH CONDITIONS BEFORE THEY ARE DISTURBED.
- 4.3 CONTRACTOR HEREBY DECLARES THAT HE HAS READ AND IS FAMILIAR WITH THE APPLICABLE SOILS REPORT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR STRICTLY ADHERING TO THE RECOMMENDATIONS OF SAID SOILS REPORT FOLLOWING ALL APPLICABLE PROCEDURES NOTED THEREIN. EXISTING WATER TABLE SHALL POSSIBLY DETERMINE A DIFFERENT UTILIZATION OF THE TANK STRAPS AND TYPE OF BACK FILL MATERIAL. SHOULD GROUND WATER OR CONTAMINATION BE DISCOVERED DURING TANK EXCAVATION, WORK SHALL BE SUSPENDED PENDING REVIEW BY 7-ELEVEN CONSTRUCTION MANAGER.
- 4.4 CONTRACTOR SHALL REPORT ANY OBJECTION TO MATERIALS, APPLIANCES, OR METHODS OF CONSTRUCTION SHOWN OR SPECIFIED TO THE OWNER'S CONSTRUCTION MANAGER AND OBTAIN A DECISION BEFORE PROCEEDING.
- 4.5 PROPOSALS: CONTRACTOR SHALL SUBMIT BID ONLY ON 7-ELEVEN'S FORM THAT WILL BE PROVIDED BY 7-ELEVEN'S LOCAL CONSTRUCTION MANAGER. "COST OF CONSTRUCTION" PROPOSALS SUBMITTED ON ANY OTHER FORM SHALL BE REJECTED AND RETURNED. PROPOSALS NOT COMPLETELY AND PROPERLY FILLED OUT SHALL BE REJECTED. PROPOSALS SHALL INCLUDE MONEYS FOR REMOVAL OF ALL EXISTING IMPROVEMENTS AS REQUIRED FOR GASOLINE INSTALLATION.
- 4.6 PERMITS: CONTRACTOR SHALL, AS DIRECTED BY 7-ELEVEN, PAY FOR ALL APPLICABLE PERMITS AND AND FEES. COPIES OF PERMITS AND CERTIFICATES SHALL BE FORWARDED TO LOCAL 7-ELEVEN CONSTRUCTION MANAGER'S OFFICE OR SITE MANAGER PRIOR TO COMMENCEMENT OF WORK. FINAL CERTIFICATION OF ACCEPTANCE BY GOVERNING AUTHORITIES SHALL ALSO BE ON FILE WITH LOCAL CONSTRUCTION OFFICE OR SITE MANAGER PRIOR TO THE PROCESSING OF FINAL PAYMENT REQUEST.
- 4.7 INSPECTIONS: CONTRACTOR SHALL BE REQUIRED TO ADHERE TO ALL REQUIREMENTS OF OWNER'S INSPECTION PROGRAM. CONTRACTOR SHALL COMPLY WITH REQUIREMENTS FOR NOTIFICATION, SITE PREPARATION REQUIREMENTS, COMPLIANCE, ON-SITE PRESENCE DURING INSPECTION AND CORRECTION OF ANY DEFECTS OR RELATED PROBLEMS AS DIRECTED BY INSPECTING REPRESENTATIVE CONTRACTOR SHALL PROVIDE NO LESS THAN 48 HOURS NOTICE PRIOR TO INSPECTION.
- 4.8 COORDINATION: FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING ALL PHASES OF THE GASOLINE INSTALLATION WITH THE GENERAL CONTRACTOR IN CHARGE OF THE BUILDING OR MAJOR PORTION OF THE PROJECT. THE BUILDING GENERAL CONTRACTOR AND THE FUEL CONTRACTOR SHALL JOINTLY DEVELOP A MUTUALLY AGREEABLE SCHEDULE AND TIMETABLE FOR COMPLETION ON THE GASOLINE INSTALLATION. TIMETABLE FOR BUILDING AND GASOLINE SHALL COINCIDE AND COMPLIMENT EACH OTHER SO THAT ONE DOES NOT DELAY THE OTHER.
- SHOULD FUEL CONTRACTOR FAIL TO MEET DEADLINES AS ESTABLISHED BY THE SCHEDULE AND/OR HOLD UP THE COMPLETION OF THE OVERALL BUILDING PROJECT, 7-ELEVEN, INC. RESERVES THE RIGHT TO RECTIFY THE SITUATION ACCORDINGLY.
- 4.9 CONTRACTOR SHALL PROVIDE OWNER WRITTEN CERTIFICATION AS TO THE FOLLOWING:
- 4.9.1 CERTIFICATION FROM LOCAL/STATE GOVERNING AUTHORITIES AS APPLICABLE FOR CONTRACTOR CERTIFICATION/LICENSING FOR INSTALLATION OF GASOLINE STORAGE TANK SYSTEM.
 - 4.9.2 CERTIFICATION FROM EQUIPMENT MANUFACTURERS AND SUPPLIERS (TANKS, PRODUCT LINES, ATG'S, ETC.) AS TO ATTENDING AND ACHIEVING CERTIFICATION FROM APPROPRIATE COMPANY FOR INSTALLATION OF EQUIPMENT.
 - 4.9.3 CERTIFICATION ON APPROPRIATE FORMAT AS TO INSTALLATION OF COMPLETE SYSTEM BEING PERFORMED IN COMPLIANCE WITH ALL LOCAL, STATE AND FEDERAL LAWS AND REGULATIONS.
 - 4.9.4 CERTIFICATION THAT ALL EQUIPMENT EITHER SUPPLIED BY OWNER OR CONTRACTOR HAS BEEN INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS BY PERSONNEL TRAINED AND QUALIFIED FOR THAT SPECIFIC ITEM.

SECTION I GENERAL CONDITIONS - CONTINUED

PART 4 - CONTINUED

- 4.11 LIENS: CONTRACTOR SHALL PERMIT NO LIENS OF ANY KIND TO BE FIXED UPON OR AGAINST THE PROPERTY BY ITS SUBCONTRACTORS, LABORERS, MECHANICS OR MATERIAL MEN, AND SHALL INDEMNIFY, PROTECT AND SAVE OWNER HARMLESS FROM AND AGAINST ALL SUCH CLAIMS AND LIENS.
- 4.12 EQUIPMENT RESPONSIBILITY: CONTRACTOR SHALL BE RESPONSIBLE FOR SCHEDULING, RECEIVING, UNLOADING, HANDLING AND PROVIDING PROTECTED INSIDE STORAGE FOR ALL OWNER SUPPLIED EQUIPMENT AND MATERIAL. CONTRACTOR SHALL INSPECT EQUIPMENT UPON RECEIPT AND IMMEDIATELY REPORT ANY DAMAGE DUE TO SHIPPING TO THE OWNER'S REPRESENTATIVE. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE DUE TO FAILURE TO COMPLY WITH THESE REQUIREMENTS. DISPENSERS SHALL BE PROTECTED WITH SHIPPING BOXES UNTIL STORE IS TURNED OVER FOR OPENING. CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ADEQUATE AND LOCKED STORAGE OF OWNER'S MISCELLANEOUS EQUIPMENT, TO INCLUDE GASOLINE CONSOLE, PRIOR TO OPENING OF THE STORE.
- 4.13 FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR FILING OF RECEIVING FORMS AND MISCELLANEOUS CONFIRMATION OF EQUIPMENT RECEIVING INFORMATION AS REQUIRED BY LOCAL CONSTRUCTION OFFICE.
- 4.14 FUEL CONTRACTOR SHALL PROVIDE ALL PIPING, WIRING, MATERIAL AND LABOR AS REQUIRED TO INSTALL OWNER SUPPLIED EQUIPMENT. INCLUDE MONEY IN BID PROPOSAL FOR INSTALLATION OF OWNER SUPPLIED EQUIPMENT UNLESS NOTED OTHERWISE.

PART 5 - PROTECTION

- 5.1 CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY LOSS OR DAMAGE CAUSED BY HIM OR HIS WORKMEN TO THE PROPERTY OR EQUIPMENT OF THE OWNER, OR TO THE WORK OR MATERIALS OF OTHER CONTRACTORS. CONTRACTOR SHALL MAKE GOOD ANY LOSS, DAMAGE OR INJURY WITHOUT COST TO OWNER FOR SUCH LOSS OR DAMAGE.
- 5.2 CONTRACTOR SHALL PROVIDE AND MAINTAIN SUITABLE TEMPORARY SIDEWALKS, FENCES, LIGHTS, SIGNS OR OTHER STRUCTURES AND DEVICES AS REQUIRED BY LAW, DO NOT OBSTRUCT OR INTERFERE WITH TRAFFIC IN PUBLIC STREETS, ALLEYS OR PRIVATE RIGHT-OF-WAYS. IF WORK IS SUSPENDED, KEEP ALL ROADWAYS AND SIDEWALKS IN PROPER CONDITION AND PUT AND LEAVE THEM IN PROPER CONDITION AT TERMINATION OF WORK.
- 5.3 CONTRACTOR SHALL SEND PROPER NOTICES, MAKE NECESSARY ARRANGEMENTS AND PERFORM ALL WORK REQUIRED FOR THE CARE, PROTECTION AND MAINTENANCE OF PUBLIC UTILITIES ON AND AROUND THE BUILDING SITE, ASSUMING ALL RESPONSIBILITY AND PAYING ALL COSTS FOR WHICH THE OWNER MAY BE LIABLE. CONTRACTOR HEREBY DECLARES HE HAS VERIFIED THE LOCATIONS OF THE EXISTING UNDERGROUND UTILITIES ON SITE PRIOR TO THE START OF WORK. CONTRACTOR HEREBY AGREES TO MAKE ARRANGEMENTS FOR AND TO PAY ALL CHARGES IN CONJUNCTION WITH THE RELOCATION'S OF EXISTING UTILITIES AS REQUIRED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL UTILITY EASEMENTS, HOOKUP CHARGES, TAP FEES & SYSTEM DEVELOPMENT FEES OR BONDS AS NECESSARY FOR COMPLETION OF THIS PROJECT.
- 5.4 IF ALL OR PART OF THE WORK IS SUSPENDED FOR ANY REASON, CONTRACTOR SHALL PROPERLY COVER OVER, SECURE AND PROTECT ANY PORTION LIABLE TO SUSTAIN INJURY FROM ANY CAUSE.
- 5.5 PRODUCT PROTECTION: CONTRACTOR SHALL PROVIDE ADEQUATE AND SECURE PROTECTION OF PRODUCT IN TANKS UPON DELIVERY UNTIL TURNED OVER FOR OPERATION. ALL FILL, VENT AND GAUGING CAPS AND/OR OPENINGS SHALL BE SECURED AND PADLOCKED TO PREVENT VANDALISM OR THEFT.

PART 6 - SUBSTITUTION OF MATERIALS

- 6.1 MATERIALS AS SPECIFIED REPRESENT REQUIRED STANDARDS. SUBSTITUTION MAY BE PROPOSED IN WRITING WITH ADEQUATE SUPPORTING DATA FURNISHED. USE OF SUBSTITUTE MATERIALS IS DEPENDENT ON RECEIPT OF WRITTEN APPROVAL FROM OWNER'S APPROVED REPRESENTATIVE.

PART 7 - COMPLETION OF CONTRACT

- 7.1 THE CONTRACT SHALL BE CONSIDERED FULFILLED, SAVE AS PROVIDED IN ANY MAINTENANCE STIPULATIONS, BOND OR BY LAW, WHEN ALL THE WORK HAS BEEN COMPLETED WITH FINAL INSPECTION AND ACCEPTANCE MADE BY ALL APPLICABLE GOVERNING BUILDING DEPARTMENTS, FIRE MARSHAL'S OR OTHER JURISDICTIONS.
- 7.2 NOT USED
- 7.3 CLEANING:
- 7.3.1 ALL WORK SHALL BE CLEAN AND READY FOR USE UPON COMPLETION. REMOVE TEMPORARY TAPES, WRAPPING, COATING, PAPER LABELS AND OTHER ITEMS.
 - 7.3.2 CLEANING METHODS FOR PROPRIETARY MATERIALS SHALL BE IN STRICT ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS. CLEANING SOLUTIONS, AGENTS, SOLVENTS, WAXES OR OTHER MATERIALS SHALL BE ONLY AS APPROVED BY THE MANUFACTURER OF THE MATERIAL INSTALLED IN THE WORK.
 - 7.3.3 CLEANUP: CONTRACTOR SHALL CLEAN UP ALL DEBRIS CAUSED BY THE WORK OF THIS SECTION, KEEPING THE PREMISES NEAT AT ALL TIMES.

PART 8 - CODES

- 8.1 THE FOLLOWING CODES ARE MADE A PART OF THIS SPECIFICATION. SAID CODES SHALL DICTATE MINIMAL ACCEPTABLE STANDARDS. CODE SHALL BE ADHERED TO UNLESS LOCAL GOVERNING AUTHORITIES DICTATE HIGHER OR MORE STRINGENT REQUIREMENTS WHICH SHALL TAKE PRECEDENCE:
- 8.1.1 NFPA 30 FLAMMABLE AND COMBUSTIBLE LIQUIDS CODE
 - 8.1.2 PEI MANUAL "RECOMMENDED PRACTICES FOR INSTALLATION OF UNDERGROUND LIQUID STORAGE SYSTEMS." #RP-100-96 (OR LATEST EDITION)
 - 8.1.3 OSHA AND LOCAL MUNICIPALITIES

PART 9 - WARRANTY / GUARANTEE

- 9.1 EXCEPT WHERE SPECIAL GUARANTEES ARE REQUIRED IN EXCESS OF ONE (1) YEAR, THE CONTRACTOR AGREES TO REPAIR AND RE-CERTIFY OR REPLACE THE DEFECTIVE COMPONENT OR ANY DEFECT IN MATERIAL OR WORKMANSHIP (BEYOND ORDINARY WEAR AND TEAR) TO THE SATISFACTION OF THE OWNER'S CONSTRUCTION MANAGER FOR A PERIOD OF ONE (1) YEAR AFTER DATE OF ACCEPTANCE, WHETHER IN HIS WORK OR IN THAT OF SUBCONTRACTOR'S, WITHOUT COST TO THE OWNER.
- 9.2 ALL PAVING WILL HAVE A TWO YEAR WARRANTY.

PART 10 - AS BUILT PLAN AND DOCUMENTATION

- 10.1 UPON COMPLETION OF PROJECT, CONTRACTOR SHALL PROVIDE "AS-BUILT SITE PLAN" VERIFYING FINAL LOCATION OF IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO, PIPING, PLUMBING AND ELECTRICAL.
- 10.2 CONTRACTOR SHALL KEEP PHOTOGRAPHIC JOURNAL OF ALL PHASES OF THE COMPLETE INSTALLATION. PHOTOGRAPHS ARE TO BE PROPERLY DATED, IDENTIFIED AND FORWARDED TO LOCAL 7-ELEVEN, INC. CONSTRUCTION OFFICE, WITH FINAL INVOICE PACKAGE.

SECTION II INSPECTIONS / TESTING

TEST BEFORE PLACING THE SYSTEM IN SERVICE			
COMPONENT	MANUFACTURER	INSTALLATION MANUAL	TESTING PROCEDURE
DOUBLE-WALLED STORAGE TANK	XERXES "A ZCL COMPANY"	INSTALLATION MANUAL http://www.zcl.com/assets/documents/library/Installation%20manual%20and%20operating%20guidelines%20(mog)%20for%20xerxes%20tanks.pdf	TESTING PROCEDURE http://www.zcl.com/assets/documents/library/Installation%20manual%20and%20operating%20guidelines%20(mog)%20for%20xerxes%20tanks.pdf
DOUBLE-WALLED PIPING	NOV	INSTALLATION MANUAL http://www.nov.com/Segments/Completion_and_Production_Solutions/Fiber_Glass_Systems/Fuel_Handling/Red_Thread_IA.aspx	TESTING PROCEDURE http://www.nov.com/docHandler.aspx?puId=12bNzWjC3oL35C
LEAK DETECTION SENSOR	VEEDER ROOT	INSTALLATION MANUAL http://www.gilbarco.com/gold/gold_public_access.cfm?section_id=361	TESTING PROCEDURE http://www.gilbarco.com/gold/gold_public_access.cfm?section_id=361
AUTOMATIC TANK GUAGES	VEEDER ROOT	INSTALLATION MANUAL https://www.veeder.com/us/sites/veeder.com.us/files/2020-09/577014-073%20TL5-450PLUS%20Console%20Site%20Prep%20and%20Installation%20Manual.pdf	TESTING PROCEDURE https://www.veeder.com/us/automatic-tank-gauging-products/tis-450plus-automatic-tank-gauge
OVERFILL PREVENTION DEVICES	OPW	INSTALLATION MANUAL http://www.opwglobal.com/docs/libraries/manuals/retail-fueli ng/below-ground/opw-fcs-manuals/71so-testable-(h15524pa)-1-30-14.pdf?sfvrsn=4	TESTING PROCEDURE http://www.opwglobal.com/docs/libraries/manuals/retail-fueli ng/below-ground/opw-fcs-manuals/71so-testable-(h15524pa)-1-30-14.pdf?sfvrsn=4
SPILL-CONTAINMENT MANHOLES	OPW	INSTALLATION MANUAL http://www.opwglobal.com/docs/libraries/manuals/retail-fueli ng/below-ground/opw-fcs-manuals/10_Series_Emergency_She ar_Valve_Installation_Instructions_-_English_H11374PA.pdf?5t atus=Master&sfvrsn=0	TESTING PROCEDURE http://www.opwglobal.com/docs/libraries/manuals/retail-fueli ng/below-ground/opw-fcs-manuals/10_Series_Emergency_She ar_Valve_Installation_Instructions_-_English_H11374PA.pdf?5t atus=Master&sfvrsn=0
IMPACT VALVES	OPW	INSTALLATION MANUAL http://www.vaporless.com/installation/installpdf/install_id200 0.pdf	TESTING PROCEDURE http://www.vaporless.com/installation/installpdf/install_id200 0.pdf
MECHANICAL LINE LEAK DETECTORS	VAPORLESS MANUFACTURING	INSTALLATION MANUAL http://www.veeder.com/gold/download.cfm?doc_id=6414	TESTING PROCEDURE http://www.veeder.com/gold/download.cfm?doc_id=6414
ELECTRONIC LINE LEAK DETECTORS	VEEDER ROOT	INSTALLATION MANUAL http://www.franklinfueling.com/media/468560/407293001-1-flex-connector-install.pdf	TESTING PROCEDURE http://www.franklinfueling.com/media/468560/407293001-1-flex-connector-install.pdf
FLEXIBLE CONNECTORS	FRANKLIN FUELING	INSTALLATION MANUAL	

PART 1 - GENERAL

- 1.1 SCOPE: THIS SECTION COVERS THE FURNISHING OF ALL LABOR, SERVICES AND EQUIPMENT AS REQUIRED TO PROVIDE INSPECTION SERVICES OF THE FUELING INSTALLATION.

PART 2 - INSPECTIONS

- 2.1 INSPECTOR: 7-ELEVEN, INC. LOCAL CONSTRUCTION DEPARTMENT AND/OR APPOINTED REPRESENTATIVE SHOULD BE NOTIFIED TO OBSERVE TIGHTNESS TESTING OF PRODUCT PIPING, STAGE I & STAGE II PIPING PRIOR TO BACKFILL.
- 2.2 NOTIFICATION: CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING 7-ELEVEN'S CONSTRUCTION DEPARTMENT 48 HOURS PRIOR TO REQUESTED INSPECTION
- 2.3 SPILLAGE: SHOULD SPILLAGE OF PRODUCT OCCUR AT ANY PHASE OF INSTALLATION OR TESTING, LOCAL 7-ELEVEN CONSTRUCTION DEPARTMENT AND ENVIRONMENTAL DEPARTMENT ARE TO BE NOTIFIED IMMEDIATELY, ESTIMATED LOSS OF PRODUCT AS WELL AS ACTION TO BE TAKEN BY CONTRACTOR TO SAFEGUARD THE EVENT FROM FURTHER MISHAP AND TO PROTECT THE GENERAL PUBLIC IS TO BE NOTED AND RECORDED.
- 2.4 REPRESENTATIVE OF CONTRACTOR AND FUEL CONTRACTOR SHALL BE AVAILABLE ON SITE DURING TESTING PROGRAMS. FAILURE TO COMPLY WITH THIS REQUIREMENT WILL RESULT IN ANY CORRECTIONS AND/OR REPAIRS REQUIRED BEING PERFORMED BY OWNER AS REQUIRED. COST OF SAID REPAIRS AND/OR CONNECTIONS SHALL BE DEDUCTED FROM CONTRACT AMOUNT AS REQUIRED TO SATISFY THE SITUATION.
- 2.5 PROHIBITED WORK: CONTRACTOR SHALL SCHEDULE WORK SUCH THAT NO HEAVY EQUIPMENT IS OPERATING ON SITE DURING INSPECTION PROGRAMS. NO WELDING OR OPEN FLAMES SHALL BE ALLOWED ON SITE AT ANY TIME DURING TEST PROCEDURES.
- 2.6 INSTALLATION REPORTS: SHOULD 7-ELEVEN OR THEIR APPOINTED REPRESENTATIVES BE UNABLE TO BE PRESENT AT SITE FOR PURPOSES OF FILING ANY INSPECTION REPORTS, CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE INSPECTION AND FILING OF REPORT WITH LOCAL 7-ELEVEN, INC. CONSTRUCTION OFFICE. WRITTEN REPORT SHALL INCLUDE PHOTOGRAPHIC DOCUMENTATION OF ALL PROCEDURES.
- 2.7 TEST FAILURE: SHOULD SYSTEM FAIL ANY PORTION OF TEST PROGRAM, CONTRACTOR SHALL IMMEDIATELY INSTIGATE PROCEDURES AS REQUIRED TO BRING SYSTEM INTO COMPLIANCE SHOULD FAILURE OF TEST DUE TO FACILITY WORK REQUIRE EXPENDITURE OF ADDITIONAL MONIES FOR RESCHEDULING AND/OR RETESTING OF SYSTEM, CONTRACTOR SHALL BE HELD RESPONSIBLE FOR COMPENSATION ACCORDINGLY.
- 2.8 CLEANUP: IN THE EVENT PRODUCT SPILLAGE SHOULD OCCUR DURING CONSTRUCTION OR DURING WARRANTY PERIOD DUE TO WORK PERFORMED BY CONTRACTOR, CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANUP AS MAY BE REQUIRED.
- 2.9 TEST REPORT: UPON COMPLETION OF TESTING PROGRAMS, REPRESENTATIVE OF TESTING/INSPECTION COMPANY SHALL BE RESPONSIBLE FOR FILING COPIES OF TANK AND PIPING TEST/INSPECTION REPORTS WITH LOCAL 7-ELEVEN, INC. CONSTRUCTION OFFICE OR THE OWNER'S ASSIGNED SITE MANAGER WITH COPY TO JOB SITE.

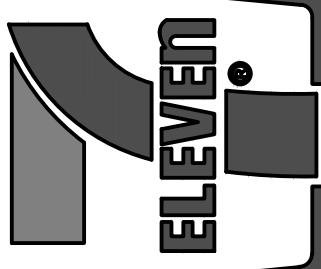
Description

Date

Rev. #

Proto 2022-04

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
WAPPINGERS FALLS, NY 12590
FUELING SPECIFICATIONS



CORE STATES GROUP

CONCRETE ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 000079
46 East Main Street
Suite 200
Suffolk, VA 23063
908-462-9700
core-states.com

Job#: SPW-36100-SEI
Scale: AS NOTED
Date: 02/14/23
Drawn By: RMC
Checked By: CGC

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FUELING - USA

PERFORMANCE SPECIFICATION - CONT.

SECTION II INSPECTIONS / TESTING - CONTINUED

PART 3 - TANK INSTALLATION INSPECTION

- 3.1 SCHEDULE: CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING SCHEDULE SO TANKS ARE TESTED AND SET IN TANK HOLE SAME DAY OF DELIVERY.
- 3.2 EXCAVATION: ALL EXCAVATIONS SHALL BE COMPLETED WITH BEDDING MATERIAL IN PLACE AND READY FOR INSTALLATION PRIOR TO TANK DELIVERY.
- 3.3 ANCHORING: CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE INSTALLATION OF CONCRETE ANCHORS (DEAD MAN).
- 3.4 STRAPS: HOLD DOWN STRAPS TO BE INSTALLED AS PER MANUFACTURER'S SPECIFICATIONS - MINIMUM OF 4 - MORE IF REQUIRED BY MANUFACTURER (STRAPS SHALL BE SHIPPED WITH APPROPRIATE PADDING MATERIAL).
- 3.5 AIR TEST: TANKS SHALL BE AIR TESTED AS PER MANUFACTURER'S SPECIFICATIONS PRIOR TO PLACEMENT. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL MATERIALS TO PROVIDE COMPLETE AIR TEST INCLUDING, BUT NOT LIMITED TO, AIR COMPRESSOR OF SUFFICIENT SIZE AND CAPACITY, SPECIFIED SOAP MATERIAL AND ACCEPTABLE SOAP APPLICATION EQUIPMENT.
- 3.6 MINIMUM TESTING: DOUBLE WALL FIBERGLASS TANKS SHOULD ARRIVE AT THE JOB SITE WITH A MINIMUM OF 4" HG ON THE INTERSTICE. OBSERVE THE VACUUM HOLD TEST FOR AT LEAST ONE (1) HOUR PRIOR TO SETTING THE TANKS. SHOULD THE INTERSTICE NOT HOLD A MINIMUM 4" HG VACUUM, THEN CONTACT THE TANK MANUFACTURER IMMEDIATELY.
- 3.7 OBSERVATION: 7-ELEVEN'S CONSTRUCTION DEPARTMENT AND/OR REPRESENTATIVE SHALL BE RESPONSIBLE FOR INSPECTION OF AIR OR VACUUM TEST, OBSERVATION OF LIFTING, SETTING AND BURIAL OF TANKS.
- 3.8 TANK INSPECTION REPORT: WRITTEN REPORT SHALL INCLUDE PHOTOGRAPHIC DOCUMENTATION OF ALL PROCEDURES.

PART 4 - TANK AND LINE TESTING

- 4.1 CONTRACTOR SHALL COMPLETE AND BE RESPONSIBLE FOR THE FOLLOWING PRIOR TO FINAL PLACEMENT OF CONCRETE AND FINAL SYSTEM TEST:
 - 4.1.1 PIPING: ALL PIPING SHALL BE COMPLETED EXCEPT INSTALLATION OF OVERFILL PROTECTION. OVERFILL PROTECTION SHALL BE INSTALLED UPON COMPLETION OF TEST.
 - 4.1.2 VENTS: VENT LINES SHALL BE COMPLETED UNDERGROUND AND STUBBED UP MINIMUM OF 12' ABOVE FINISHED GRADE.
 - 4.1.3 PRODUCT LINES: ALL PRODUCT LINES SHALL BE COMPLETE WITH EMERGENCY SHUTOFF IMPACT VALVE INSTALLED AT DISPENSER BOX WITH MECHANICAL LEAK DETECTOR IN PLACE.
 - 4.1.4 PUMPS: SUBMERSIBLE PUMPS AND LINES SHALL BE COMPLETELY INSTALLED.
 - 4.1.6 SITE PREPARATION: CONTRACTOR SHALL PREPARE TANK AREA FOR WATER OR PRODUCT DELIVERY AND TANK TESTING PROCEDURES.
 - 4.1.7 FILL: CONTRACTOR SHALL PROVIDE 42" MINIMUM BACK FILL OVER TANKS.
 - 4.1.8 WATER: SHOULD WATER BE PRESENT IN TANK HOLE, CONTRACTOR SHALL VERIFY AND NOTIFY 7-ELEVEN CONSTRUCTION MANAGER OF STATUS.
- 4.2 AFTER PLACEMENT OF CONCRETE, A COMPLETE SYSTEM TIGHTNESS TEST INCLUDING TANKS, PRODUCT LINES, STAGE II VAPOR RECOVERY LINES, SECONDARY CONTAINMENT PIPING AND VENT LINES SHALL BE PERFORMED. TESTING SYSTEM SHALL BE AS APPROVED BY 7-ELEVEN. 7-ELEVEN WILL BE BILLED DIRECTLY FOR TESTS. CONTRACTOR TO COORDINATE & SCHEDULE. RE-TEST OR STAND AROUND TIME BY THE TESTING COMPANY TO BE BILLED TO CONTRACTOR. DETAILS FOR THIRD PARTY TESTING REQUIREMENTS LISTED ON G17.0 SHEETS.
 - 4.2.1 AFTER ALL UNDERGROUND PIPING AND ELECTRIC CONDUITS ARE INSTALLED AND DURING EXCAVATION BACKFILLING AND CONCRETE POURING, A CONTINUOUS HYDROSTATIC TEST SHALL BE PERFORMED ON ALL SUMPS & SPILL BUCKETS TO ENSURE TIGHTNESS AND PROPER INSTALLATION.
- 4.3 PRODUCT DELIVERY: CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFICATION TO 7-ELEVEN REPRESENTATIVE OF TEST DATE AND VERIFICATION OF NEED FOR TANKS TO BE FILLED WITH PRODUCT AND TANK MONITORING SYSTEM TO BE OPERATIONAL FOR TESTING PROCEDURES. A MINIMUM OF 72 HOURS ADVANCE NOTIFICATION WILL BE REQUIRED FOR PRODUCT DELIVERY. CHECK WITH LOCAL REGULATORY AGENCIES FOR ANY FURTHER REQUIREMENTS.
- 4.4 CONTRACTOR SHALL HAVE REPRESENTATIVES ON SITE TO ACCEPT AND RECORD ALL PRODUCT DELIVERIES. CONTRACTOR'S REPRESENTATIVE SHALL REMIND DELIVERY COMPANY THAT NO OVERFILL PROTECTION IS INSTALLED AND TO FOLLOW PROCEDURES FOR DELIVERY AS SITUATION DICTATES.
- 4.5 PRODUCT REMOVAL: ANY REMAINING PRODUCT IN VENT LINES UPON COMPLETION OF TEST SHALL BE REMOVED BY TESTING COMPANY.

PART 5 - STARTUP INSPECTION

- 5.1 SCHEDULING: CONTRACTOR SHALL BE RESPONSIBLE FOR FINAL SCHEDULING STARTUP AND CHECK-OUT. CONTRACTOR SHALL NOTIFY LOCAL CONSTRUCTION DEPARTMENT AND APPLICABLE MAINTENANCE DEPARTMENT OFFICE A MINIMUM OF 48 HOURS PRIOR TO INTENDED FINAL STARTUP AND CHECK-OUT DATE AND TIME.
- 5.2 PERSONNEL: CONTRACTOR, FUEL SUBCONTRACTOR AND ELECTRICAL SUBCONTRACTOR SHALL BE AVAILABLE ON-SITE DURING STARTUP, AS APPROVED BY 7-ELEVEN, INC. ALL STARTUPS ARE TO BE DONE BY AN AUTHORIZED MANUFACTURER'S CONTRACTOR FOR THAT PIECE OF EQUIPMENT.
- 5.3 STARTUP: PHYSICAL STARTUP AND CHECK-OUT OF SYSTEMS AND EQUIPMENT SHALL BE DONE BY AND AT THE DIRECTION OF 7-ELEVEN REPRESENTATIVE ONLY. CONTRACTOR SHALL NOT START OR ACTIVATE ANY EQUIPMENT WITHOUT THE SPECIFIC DIRECTIVE OF 7-ELEVEN REPRESENTATIVE. DAMAGE TO ANY 7-ELEVEN EQUIPMENT DUE TO IMPROPER INSTALLATION AND/OR UNAUTHORIZED STARTUP WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 5.4 SITE COMPLETION: ALL SITE WORK, INCLUDING CONCRETE/ASPHALT PAVEMENTS, SHALL BE COMPLETE. ALL UTILITIES SHALL BE CONNECTED AND OPERATIONAL. ALL LANDSCAPING, INCLUDING SPRINKLER SYSTEMS, SHALL BE COMPLETE.
- 5.5 VERIFICATION: PRIOR TO SCHEDULING OF STARTUP, CONTRACTOR SHALL OBSERVE AND VERIFY CORRECTNESS AND COMPLIANCE TO 7-ELEVEN'S DRAWINGS AND SPECIFICATIONS FOR THE FOLLOWING, BUT THESE SHALL NOT COMPRISE THE TOTAL LIMIT OF THE CONTRACTOR'S VERIFICATION:
 - 5.5.1 MANHOLES SHALL BE PROPERLY CENTER/INSTALLED AT OVERFILLS, SUBMERSIBLES, VAPOR RECOVERY, AUTO GAUGING, OBSERVATION WELLS AND SUMP/RISER.
 - 5.5.2 PROPER HEIGHT OF GRAVEL BACK FILL AROUND SUMP/RISERS.
 - 5.5.3 TANK I.D. MARKERS INSTALLED IN CONCRETE AT PROPER LOCATIONS.

SECTION II INSPECTIONS / TESTING - CONTINUED

PART 5 - STARTUP INSPECTION - CONTINUED

- 5.5.4 ALL BRASS GOODS AND FITTINGS PROPERLY INSTALLED, TIGHT AND SECURE.
- 5.5.5 LEAK DETECTORS INSTALLED.
- 5.5.6 PROTECTED FLEX CONNECTORS ARE PROPERLY INSTALLED WITHOUT TWIST IN INSTALLATION.
- 5.5.7 MANHOLE EXTENDERS PROPERLY INSTALLED AND SCREWED IN FRAME OF MANHOLE SUMP/RISERS, PROPERLY SET AND SEALED TO TANK MAN WAY OR FITTINGS AND CLEANED OUT FREE OF DEBRIS AND CONCRETE.
- 5.5.8 TANKS CLEAN AND FREE OF WATER. PRIOR TO PRODUCT FILL, TANKS ARE TO BE CHECKED FOR ANY DEBRIS AND WATER. ANY EVIDENCE OF BOTH, TANKS ARE TO BE CLEANED AND WATER REMOVED.
- 5.5.9 ISLAND FORM INNER BOXES PROPERLY CLEANED OUT FREE OF DEBRIS AND CONCRETE.
- 5.5.10 DISPENSERS PROPERLY SET AND SECURED TO ISLAND BOX FRAME AND DISPENSER SUMPS PROPERLY CLEANED OUT FREE OF DEBRIS AND CONCRETE.
- 5.5.11 SHEAR VALVES SECURED TO FRAME WITH PROPER HARDWARE.
- 5.5.12 PHASE II VAPOR RECOVERY LINES STUBBED UP AND SECURED IN PROPER LOCATION.
- 5.5.13 ELECTRICAL CONDUITS SECURED WITH ALL CONNECTIONS COMPLETED. VERIFY COMPLIANCE WITH REQUIREMENTS FOR ISOLATED GROUND WIRING TO JUNCTION BOX CASING.
- 5.5.14 DISPENSER CANOPIES INSTALLED AND LIFTING LUGS REMOVED WITH PROPER PLUGS INSERTED.
- 5.5.15 ALL NOZZLES AND SWIVELS AVAILABLE ON-SITE, INSTALLED PRIOR ONLY IF REQUIRED BY GOVERNING AUTHORITIES FOR INSPECTION PURPOSES.
- 5.5.16 ALL INSPECTIONS BY GOVERNING AUTHORITIES COMPLETED AND SIGNED. SIGNED AND COMPLETED PERMIT CARD ON SITE.
- 5.5.17 ELECTRICAL DEVICES INCLUDING LIGHTS, SIGNS AND INTERCOMS PROPERLY INSTALLED, SECURED AND PLUMB.
- 5.5.18 ANY WIRING INSPECTION PORTS IN CANOPY COLUMNS COVERED WITH ACCEPTABLE PLATES.
- 5.5.19 VENT LINES PROPERLY SECURED, PROTECTED AND INSTALLED AT PROPER HEIGHT.
- 5.5.20 VERIFICATION OF COMPLIANCE FOR TANK AND LINE TEST AVAILABLE ON SITE.
- 5.5.21 ALL MANHOLES FREE OF CONCRETE AND DEBRIS.
- 5.5.22 MANHOLE COVERS PROPERLY PAINTED.
- 5.5.23 ALL PAINTING COMPLETE INCLUDING CANOPY, DOWN SPOUTS, METALS AND MISCELLANEOUS METALS.
- 5.5.24 CONCRETE AT ISLANDS AND TANK PAD TRUE, PROPERLY SLOPED AND FINISHED WITH ACCEPTABLE RISE TO MANHOLES.
- 5.5.25 ELECTRICAL PANELS PROPERLY COMPLETED TO INCLUDE:
 - (A) ONE BREAKER FOR EACH SUBMERSIBLE PUMP, DISPENSER, CONSOLE AUTO GAUGING, DISPENSER LIGHTS (ALL) AND CANOPY LIGHTS (ALL).
 - (B) GAS PANEL CONTROLLED BY A MAIN BREAKER.
 - (C) CONTROL BOXES AND DISTRIBUTION BOXES INSTALLED AND WIRED.
 - (D) INTERCOM INSTALLATION COMPLETE AND PROPERLY WORKING.
 - (E) ALL APPLICABLE CONSOLE INSTALLATION COMPLETE AND FUNCTIONAL.
 - (F) ALL WIRING PROPERLY IDENTIFIED WITH COLOR CODED WIRING.
 - (G) CONDUITS PROPERLY IDENTIFIED. (H) ALL BREAKERS IN "OFF" POSITION.
- 5.6 NOTIFICATION: CONTRACTOR SHALL VERIFY ITEMS AS INDICATED ABOVE PRIOR TO SCHEDULING OF STARTUP MONIES AND/OR TIME LOST AT STARTUP DUE TO NON-COMPLIANCE SHALL BE CHARGED TO CONTRACTOR ACCORDINGLY.
- 5.7 EQUIPMENT SHALL BE TESTED PER SPECIFICATIONS ON SHEET G17.0.
- 5.8 CLEANUP: CLEAN UP ALL DEBRIS CAUSED BY WORK OF THIS SECTION, KEEPING PREMISES CLEAN AND NEAT AT ALL TIMES.

SECTION III EARTHWORK

PART 1 - GENERAL

- 1.1 SCOPE: THIS SECTION COVERS THE FURNISHING OF ALL LABOR, MATERIALS, SERVICES, EQUIPMENT AND APPLIANCES REQUIRED IN CONJUNCTION WITH OR PROPERLY INCIDENTAL TO EARTHWORK.

PART 2 - PRODUCTS

- 2.1 FILL MATERIAL: SHALL BE MATERIAL IN COMPLIANCE WITH TANK AND PIPING MANUFACTURER'S SPECIFICATIONS.
 - 2.1.1 PEA GRAVEL: SHALL BE CLEAN, NATURALLY ROUNDED AGGREGATE WITH A MIX OF PARTICLE SIZE NOT LESS THAN 1/8" OR MORE THAN 3/4".
 - 2.1.2 STONE OR GRAVEL CRUSHING: SHALL BE WASHED MATERIAL WITH ANGULAR PARTICLE SIZE NOT LESS THAN 1/8" OR MORE THAN 1/2".
 - 2.1.3 BACKFILL OR OTHER MATERIAL MUST BE APPROVED BY TANK MANUFACTURER.
- NOTE: MATERIALS MUST MEET ASTM C-33 PARAGRAPH 7.1 FOR QUALITY AND SOUNDNESS. FILL MATERIAL SHALL NOT HAVE MORE THAN 3% PASSING A #8 SIEVE. DRY GRAVEL DENSITY MUST BE A MINIMUM OF 95 POUNDS PER CUBIC FOOT.

SECTION III EARTH WORK - CONTINUED

PART 3 - EXECUTION

- 3.1 PROTECTIONS: PROVIDE ADEQUATE SHORING, BRACING, PILING AND CRIBBING. COMPLY WITH ALL OSHA AND LOCAL REGULATIONS AS IT PERTAINS TO SAFETY.
- 3.2 LAYOUT: CONTRACTOR SHALL BE RESPONSIBLE FOR LAYOUT AS PRESENTED IN PERMITTED PLANS AND SPECIFICATIONS.
- 3.3 EXCAVATION: EXCAVATE TO LINES, ELEVATIONS AND LIMITS REQUIRED BY THE DRAWINGS, PLUS SUFFICIENT DISTANCE AND SPACE TO PERMIT INSTALLATION OF TANKS. EXCAVATE AS REQUIRED, REGARDLESS OF TYPE, CONDITION OR MOISTURE CONTENT OF THE MATERIAL ENCOUNTERED. HAUL EXCESS MATERIAL OFF THE SITE AND DISPOSE OF SAME. DIMENSIONS OF TANK EXCAVATION SHALL BE MINIMAL AS ESTABLISHED BY 7-ELEVEN DRAWINGS UNLESS TANK MANUFACTURER REQUIREMENTS ARE GREATER. IF EXCAVATION MATERIAL IS FROM AN EXISTING GASOLINE FACILITY ALL MATERIALS MUST BE TESTED PRIOR TO REMOVAL.
- 3.4 SAW CUTTING: IF PAVING MUST BE REMOVED, THE PERIMETER OF THE TANK EXCAVATION AND ALL TRENCHES SHALL BE CUT WITH A CONCRETE SAW. NOTE: PAVING TO BE CUT 2' BEYOND EDGES OF EXCAVATIONS IN ALL DIRECTIONS IN ORDER TO GIVE PROPER SUPPORT TO NEW SLAB TO AVOID SETTLING.
- 3.5 HAZARDOUS MATERIAL: SHOULD ROCK, WATER OR OTHER HAZARDOUS MATERIALS NOT SHOWN ON SOILS TEST BE ENCOUNTERED, LOCAL CONSTRUCTION OFFICE SITE MANAGER SHALL BE IMMEDIATELY CONTACTED FOR APPROVAL TO PROCEED.
- 3.6 PROTECTION: CONTRACTOR SHALL PROVIDE SUFFICIENT PROTECTION WITH EARTHEN BERMS AT ALL TIMES TO PROTECT TANK EXCAVATION AND TRENCHES FROM DRAINAGE OF SURFACE WATERS. EXCAVATIONS SHALL BE PROPERLY MARKED, PROTECTED AND BARRICADED FOR SAFETY UNTIL BACK FILL IS COMPLETE AND SURFACING FINISHED.
- 3.7 SHORING: CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING REQUIREMENTS FOR SHORING AND PROVIDING SHORING. SHOULD SITE AND/OR SOIL CONDITIONS WARRANT SHORING. REFERENCE SOIL REPORT FOR CONDITIONS WARRANTING SPECIAL REQUIREMENTS.
- 3.8 FILLING AND BACK FILLING: PLACE APPROVED FILL MATERIAL IN STRICT ACCORDANCE WITH TANK AND PIPING MANUFACTURER'S SPECIFICATIONS. PROVIDE A 12" BED OF APPROVED BACK FILL OVER BOTTOM OF EXCAVATION PRIOR TO TANK PLACEMENT. BACK FILL SHALL BE PLACED AND COMPACTED IN MAXIMUM 12" LIFTS.
- 3.9 CLEANUP: CLEAN UP ALL DEBRIS CAUSED BY WORK OF THIS SECTION, KEEPING PREMISES CLEAN AND NEAT AT ALL TIMES.

SECTION IV TANK INSTALLATION

PART 1 - GENERAL

- 1.1 SCOPE: THIS SECTION COVERS THE FURNISHING OF ALL LABOR, MATERIALS, SERVICES, EQUIPMENT AND RELATED APPLIANCES REQUIRED IN CONJUNCTION WITH OR PROPERLY INCIDENTAL TO THE INSTALLATION OF FUEL STORAGE TANKS.

PART 2 - PRODUCTS

- 2.1 TANKS: SHALL BE DESIGNED AND MANUFACTURED SO AS TO BE CORROSION PROTECTED PER EPA REQUIREMENTS. APPROVED TANKS SHALL BE:
 - 2.1.1 DOUBLE WALL FIBERGLASS TANKS
- 2.2 HOLD DOWN STRAPS: SHALL BE SUPPLIED BY TANK MANUFACTURER ONLY. FIELD FABRICATED UNITS ARE NOT ACCEPTABLE.

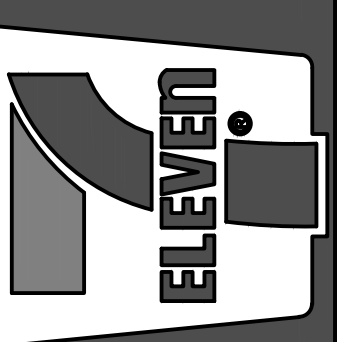
PART 3 - INSTALLATION

- 3.1 MANUFACTURING: REFER TO TANK MANUFACTURER'S CRITERIA AND SPECIFICATIONS FOR TANK INSTALLATION AND ADHERE TO AS REQUIRED.
- 3.2 CLEARANCES: MINIMUM CLEARANCE FOR TANKS SHALL BE AS FOLLOWS: SHOULD REQUIREMENTS OF TANK MANUFACTURER OR SOIL CONDITIONS DICTATE GREATER CLEARANCE, ADHERE TO ACCORDINGLY.
 - 3.2.1 BOTTOM: PROVIDE MINIMUM OF 12" BEDDING MATERIAL BETWEEN BOTTOM OF EXCAVATION OR PAD AND BOTTOM OF TANK.
 - 3.2.2 SIDES: PROVIDE MINIMUM OF 20" BETWEEN TANKS AND 3' FROM SIDE/END OF TANK TO WALL OF EXCAVATION.
 - 3.2.3 TOP: PROVIDE 42" FILL MATERIAL OVER TANK PLUS 8" OF CONCRETE
- 3.3 TESTING: PRIOR TO INSTALLATION OF TANKS, TESTING PER MANUFACTURER'S INSTRUCTIONS SHALL BE PERFORMED ON TANK(S). ON DUAL WALL TANKS, CONSULT TANK MANUFACTURER FOR TESTING CRITERIA.
- 3.4 PLACEMENT:
 - 3.4.1 REFER TO SITE PLAN FOR TANK PLACEMENT AND PRODUCT ROTATION.
 - 3.4.2 PRIOR TO SETTING OF TANKS, REMOVE ALL FOREIGN DEBRIS, ROCKS, CLOUDS, GARBAGE, ETC. FROM EXCAVATION.
 - 3.4.3 PLACE TANKS ONLY ON APPROVED FILL. DO NOT SET ON CONCRETE OR WOOD.
 - 3.4.4 NOT USED.
 - 3.4.5 TANKS SHALL BE BURIED TO MINIMUM DEPTH AS PER ABOVE AND AS SHOWN ON FUELING DRAWINGS. TANKS MAY REQUIRE GREATER BURIAL DEPTH IF INSTALLED IN REMOTE POSITION TO ALLOW FOR PROPER SLOPE ON VENT LINES VAPOR LINES. SECONDARY CONTAINMENT PRODUCT LINES. VERIFY DEPTH AS REQUIRED. (IF BURIAL DEPTH EXCEEDS 5'-0" (60"), NOTIFY 7-ELEVEN MANAGER)

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Description	
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Proto 2022-04	
7-ELEVEN, INC. 3200 HACKBERRY ROAD, IRVING TEXAS 75063 7-ELEVEN #46895 - SPEEDWAY #7645 1542 ROUTE 9 WAPPINGERS FALLS, NY 12590 FUELING SPECIFICATIONS	



CORE STATES
GROUP
CONCRETE AND ENGINEERING P.C.
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 092855

46 East Main Street
P.O. Box 90
Saybrook, NJ 08876
908.462.9700
core-states.com

Job#: SPW-36100-SEI	Scale: AS NOTED	Date: 02/14/23	Drawn By: RMC	Checked By: CGC
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PERFORMANCE SPECIFICATION - CONT.

SECTION IX CANOPIES - CONTINUED

PART 2 - EXECUTION

- 2.3 FASCIA:
- 2.3.1 VERTICAL SUPPORTS SHALL BE DESIGNED AND SPACED PER ENGINEERED (DESIGNED TO LOCAL CODES) STRUCTURAL DRAWINGS SUPPLIED BY CANOPY MANUFACTURER. SCREWS FOR FASCIA SHALL BE CARBON STEEL CADMIUM PLATED OR GALVANIZED - SIZE AS REQUIRED.
- 2.3.2 REFER TO PROJECT DRAWINGS FOR FASCIA MATERIAL AND INSTALLATION.
- 2.3.3 MATERIAL SHALL BE ONE OF THE FOLLOWING:
- A. PRE FINISHED METAL FASCIA SYSTEM.
- B. PRE FINISHED FASCIA MATERIAL AS SELECTED.

PART 3 - GENERAL

- 3.1 FABRICATION DRAWINGS:
- 3.1.1 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ENGINEERED AND STAMPED SHOP DRAWINGS FOR OVERHEAD STRUCTURE AND FOUNDATION SYSTEMS.
- 3.1.2 DRAWINGS SHALL SHOW PROJECT LOCATION AND ADDRESS.
- 3.1.3 ORIGINAL ENGINEER'S STAMP FOR APPROPRIATE STATE SHALL BE AFFIXED TO EACH PAGE OF DRAWINGS.
- 3.1.4 SHOP DRAWINGS SHALL SPECIFICALLY CALL OUT SOILS REPORT NUMBER AND DATE FOR WHICH CANOPY FOOTINGS WERE DESIGNED.
- 3.1.5 SHOP DRAWINGS SHALL SPECIFICALLY CALL OUT SIZE, WEIGHT, TYPE AND NUMBER OF ALL STRUCTURAL MEMBERS.
- 3.1.6 ALL DESIGN LOADS SHALL COMPLY WITH CURRENT BUILDING CODES AND LOCAL BUILDING DEPARTMENT REQUIRED LOADINGS, WHICHEVER IS MOST STRINGENT. MINIMUM DESIGN LOADS SHALL BE CONSIDERED TO BE: WIND 100#, LIVE 35#, UPLIFT 35# AND DEAD LOAD 35# OR AS LOCAL STRUCTURAL DESIGN DICTATES. IF APPLICATION IS SUBJECT TO HIGH ALTITUDE (SNOW) LOADING, DESIGN SHALL BE ADJUSTED TO COMPLY ACCORDINGLY.
- 3.1.7 DEFLECTION OF ALL STRUCTURAL MEMBERS SHALL BE LIMITED TO SPAN/180 FOR L.L. ONLY.
- 3.1.8 ATTACH DECK STRUCTURAL SUPPORTS PER ENGINEERED DRAWINGS.
- 3.1.9 ROOF DECK SHALL BE AN ICC-ES APPROVED DIAPHRAGM DECK AND ATTACHED IN A MANNER THAT COULD BE CONSIDERED A DIAPHRAGM. AN ALTERNATE TO THIS IS 1/2" DIAMETER STEEL ROD DIAGONAL X-BRACING WITH TURNBUCKLES. LAYOUT OF ROD BRACING AND CONNECTIONS TO STRUCTURAL MEMBERS SHALL BE APPROVED BY STRUCTURAL ENGINEER.
- 3.1.10 CONCRETE FOOTINGS SHALL BE MADE WITH STONE AGGREGATE AND SHALL HAVE A COMPRESSIVE STRENGTH TO A MINIMUM 3000 LBS. PER SQUARE INCH WITHIN 28 DAYS AFTER POURING.
- 3.1.11 ALL FOUNDATION METAL REINFORCEMENT SHALL BE OF FORMED TYPE BARS (EXCEPT #2 BARS) AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATION A.S.T.M. 615 GRADE 40#.
- 3.1.12 THE SHOP DRAWINGS SHALL EXPLICITLY SHOW LOCATION OF DOWN SPOUTS, INTERIOR PVC DRAINS IN WARM WEATHER AREAS, ETC., AS RELATED TO ANY AND ALL UTILITIES AND/OR PIPING AT COLUMN LOCATIONS. ROOF DRAINS AND LEADERS SHALL BE DIRECTED DOWN THE INTERIOR CANOPY COLUMN EXITING SIDE OF ISLAND LEVEL OR ABOVE FINISH GRADE OF CONCRETE SIDE PAD ON DOWNHILL SIDE OF ISLAND.
- 3.1.13 INCLUDE IN DESIGN PROVISION FOR OVERFLOW DEVICE AT OPPOSITE CORNERS OF CANOPY.
- 3.1.14 CONSTRUCTION NOT SPECIFICALLY INDICATED SHALL BE ACCOMPLISHED PER MINIMUM REQUIREMENTS OF THE UNIFORM BUILDING CODE, LATEST EDITION.
- 3.1.15 IF THERE ARE ANY CONFLICTS BETWEEN LINES ON DRAWINGS AND GENERAL NOTES OR WITH SPECIFICATIONS, THE MOST STRINGENT REQUIREMENTS GOVERN.
- 3.1.16 ANY DEVIATION FROM THE PLANS OR SPECIFICATIONS SHALL BE APPROVED BY 7-ELEVEN, INC.'S CONSTRUCTION REPRESENTATIVE.
- 3.1.17 FABRICATION DRAWINGS SHALL BE DELIVERED TO APPLICABLE CONSTRUCTION OFFICE A MINIMUM OF 4 WEEKS PRIOR TO COMMENCEMENT OF CONSTRUCTION. NO WORK SHALL COMMENCE PRIOR TO APPROVAL OF DRAWINGS BY 7-ELEVEN, INC.'S CONSTRUCTION REPRESENTATIVE.

SECTION IX CANOPIES - CONTINUED

PART 3 - GENERAL - CONTINUED

- 3.2 EXECUTION:
- 3.2.1 COORDINATE DELIVERY AND INSTALLATION WITH FUEL CONTRACTOR THROUGH LOCAL CONSTRUCTION OFFICE. MATERIALS SHALL BE STORED AT POSITION AS INDICATED BY FUEL CONTRACTOR.
- 3.2.2 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING FOOTING ANCHOR BOLTS AND TEMPLATE FOR DELIVERY TO FUEL CONTRACTOR 2 WEEKS PRIOR TO INSTALLATION. SIZE IN ACCORDANCE WITH SHOP DRAWINGS AND IN ACCORDANCE WITH FUEL CONTRACTOR'S SCHEDULE OF CONSTRUCTION.
- 3.2.3 FOOTINGS FOR CANOPY AND PLACEMENT OF ANCHOR BOLTS SHALL BE PERFORMED BY FUEL CONTRACTOR AS PER APPROVED SHOP DRAWINGS. TYPE OF CONCRETE SHALL BE AS PER SHOP DRAWINGS. TYPE, SIZE, AND PLACEMENT OF ALL REINFORCED STEEL SHALL BE AS PER SHOP DRAWINGS.
- 3.2.4 THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS BEFORE STARTING WORK AND NOTIFY 7-ELEVEN, INC.'S CONSTRUCTION MANAGER IMMEDIATELY OF ANY DISCREPANCIES FOUND. REFER TO SITE PLAN FOR SIZE, TYPE AND PLACEMENT OF CANOPY
- 3.2.5 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR BEING PROPERLY LICENSED IN LOCAL INSTALLATION AS WELL AS PROVIDING OWN PERMITS AS MAY BE REQUIRED. A COPY OF THE PERMIT MUST BE ON FILE IN THE LOCAL CONSTRUCTION OFFICE PRIOR TO COMMENCING WORK.
- 3.2.6 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING CONSTRUCTION OFFICE WITH ACCEPTABLE INSURANCE AS REQUIRED PER 7-ELEVEN, INC. INSURANCE SPECIFICATIONS AND STANDARD CONTRACT FOR CONSTRUCTION. INSURANCE SHALL BE IN EFFECT FOR DURATION OF WORK.
- 3.2.7 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL WORK OF OTHER CONTRACTORS ALREADY IN PLACE, AS MAY BE REQUIRED.
- 3.2.8 CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING AND OBTAINING APPROVAL OF FUEL CONTRACTOR PRIOR TO PLACING OF ANY HEAVY EQUIPMENT ON TOP OF CONCRETE WORK ALREADY IN PLACE.
- 3.2.9 ALL FOOTINGS SHALL BE ON FIRM UNDISTURBED SOILS OR FILL COMPACTED TO 95% MAXIMUM DENSITY BASED ON A.S.T.M. D 1557-70. IF ANY POOR SOIL CONDITION IS DISCOVERED DURING EXCAVATION, THE DESIGN ENGINEER SHALL BE NOTIFIED IMMEDIATELY FOR SOIL BEARING VERIFICATION.
- 3.2.10 SUPPORTING COLUMNS SHALL BE PLACED AS PER 7-ELEVEN, INC. FUEL PLANS AND AS PER APPLICABLE SITE PLAN.
- 3.2.11 CANOPY SUPPORT COLUMNS SHALL BE CAPPED AT TOP.
- 3.2.12 ALL MATERIALS SHALL BE ALIGNED TRUE AND FLAT AND ALL PLANES SHALL BE FLAT, TRUE TO ESTABLISHED LINES WITH ALL SOFFITS BEING LEVEL AND VERTICAL WALLS CORRECT AND ACCEPTABLE. PARAPET FASCIA SHALL BE VERTICAL AND PLUMB. ALL INTERSECTIONS SHALL BE TRUE AND STRAIGHTEDGE LINES. MATERIALS SHALL BE LEFT IN ACCEPTABLE CONDITION TO BE PAINTED BY OTHERS. ALL METAL SHALL BE FREE OF DEBRIS AS MAY BE REQUIRED FOR FINAL PAINTING/FINISHING.
- 3.2.13 FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR NON-SHRINK GROUT BENEATH BASE PLATE AND POURING OF CONCRETE AROUND CANOPY COLUMNS, COVERING ANCHOR BOLTS AS MAY BE REQUIRED.
- 3.2.14 CANOPY CONTRACTOR SHALL LEAVE ALL EXPOSED STRUCTURAL STEEL FREE OF DEBRIS, PRIMED AND READY FOR FINAL PAINTING BY FUEL CONTRACTOR.
- 3.3 RESPONSIBILITIES OF FUEL CONTRACTOR:
- 3.3.1 SHALL BE RESPONSIBLE FOR A MINIMUM OF 30 DAY NOTIFICATION OF DATE WHEN SITE WILL BE READY FOR INSTALLATION OF GASOLINE CANOPY.
- 3.3.2 UPON AGREED DATE BETWEEN FUEL CONTRACTOR AND CANOPY CONTRACTOR, THE SITE SHALL BE FREE OF MATERIAL, MOVABLE EQUIPMENT AND DEBRIS, LEVEL AND IN PROPER CONDITION FOR CANOPY INSTALLATION. FUEL CONTRACTOR SHALL PERFORM SITE WORK AS MAY BE REQUIRED TO COMPLY.
- 3.3.3 FUEL CONTRACTOR SHALL NOT INSTALL GASOLINE DISPENSERS PRIOR TO CANOPY ERECTION, UNLESS AGREED UPON IN WRITING BETWEEN THE CANOPY CONTRACTOR AND FUEL CONTRACTOR.
- 3.3.4 GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION OF CANOPY FOOTINGS AS DESIGNED IN SHOP DRAWINGS PROVIDED BY CANOPY MANUFACTURER, PROVIDING ALL STEEL, ETC., (WITH THE EXCEPTION OF ANCHOR BOLTS TO BE PROVIDED BY CANOPY CONTRACTOR). FOOTINGS SHALL BE POURED IN A SUFFICIENT AMOUNT OF TIME, SO PROPER CURING TIME IS ALLOWED.
- 3.3.5 FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR PAINTING OF ALL METAL AND TRIM AS RELATED TO THE GASOLINE CANOPY AS PER 7-ELEVEN STORE STANDARDS MANUAL.
- 3.3.6 FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WIRING AND INSTALLATION OF LIGHTS, SIGNS, SECURITY LIGHTS, ETC., IN CONJUNCTION WITH NORMAL GASOLINE INSTALLATION. G.C. SHALL CAULK SEAL ALL LIGHT FIXTURES TO CANOPY DECK.
- 3.3.7 FUEL CONTRACTOR SHALL BE RESPONSIBLE FOR LEAVING BLOCK OUTS AS MAY BE REQUIRED FOR INSTALLATION OF CANOPY IN ACCORDANCE WITH SHOP DRAWINGS AND PLACEMENT OF ANCHOR BOLTS IN ACCORDANCE WITH SHOP DRAWINGS AS RELATED THERETO.
- 3.4 WARRANTIES: CANOPY CONTRACTOR SHALL WARRANTY, IN WRITING, COMPLETE CANOPY INSTALLATION FOR A MINIMUM PERIOD OF 2 YEARS. A 5 YEAR WARRANTY GUARANTEEING INSTALLATION TO BE FREE OF DESIGN AND INSTALLATION DEFECTS, EXCLUDING ACTS OF MAN, GOD AND NORMAL WEAR AND TEAR.
- 3.5 CLEAN-UP: CANOPY CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANUP AND REMOVAL FROM SITE ALL TRASH AND DEBRIS RESULTING FROM WORK RELATING TO INSTALLATION OF CANOPY.

SECTION X POST CONSTRUCTION TESTING REQUIREMENT

PROCEDURES FOR POST CONSTRUCTION TESTING

1.1 SCOPE OF WORK

THIRD PARTY TESTS ARE REQUIRED TO BE PERFORMED ON ANY POST CONSTRUCTION ACTIVITIES ASSOCIATED WITH NEW INSTALLATION, UPGRADE, REPLACEMENT OR REPAIR OF OR TO THE UNDERGROUND STORAGE TANK (UST) SYSTEMS, IF THE WORK INVOLVES PRODUCT PIPING, TANK SUMPS, DISPENSER SUMPS AND PANS OR SPILL CONTAINMENT BUCKETS.

TANKNOLOGY, INC IS CURRENTLY UNDER CONTRACT TO PERFORM ALL REQUIRED TESTS AS THE NATIONAL 3RD PARTY TESTING CONTRACTOR AND MUST BE USED TO PERFORM POST CONSTRUCTION TESTING TO ENSURE THAT 7-ELEVEN MAINTAINS CONTINUITY WITH THE COMPANY'S REGULATORY COMPLIANCE TESTING PROGRAM. THE ONLY EXCEPTION APPLIES IF LOCAL REGULATIONS REQUIRE THAT THE INSTALLATION CONTRACTOR PERFORM THE REQUIRED TESTS AND THE TESTS ARE WITNESSED ON SITE BY A REGULATORY ENTITY. IN THESE CASES THE INSTALLATION CONTRACTOR MUST SUBMIT DOCUMENTATION OF THE REGULATORY WITNESSED TESTS, AS WELL AS THE RESULT OF ANY SUCH TEST, DIRECTLY TO 7-ELEVEN, INC. COMPLIANCE DEPARTMENT.

IT WILL NOT BE NECESSARY FOR TANKNOLOGY TO RETEST A COMPONENT WHICH WAS TESTED BY THE CONTRACTOR, IF THAT TEST WAS WITNESSED BY A REGULATOR. IN THESE CASES THE CONTRACTOR MUST SUBMIT RESULTS OF THE TEST, AS WELL AS, COPIES OF REGULATOR INSPECTION REPORT WITHIN 48 HOURS OF COMPLETION OF THE TEST TO THE ADDRESS LISTED BELOW.

THE REQUIRED TESTS ARE DIVIDED INTO TWO CATEGORIES, PRE-FUEL TEST AND POST FUEL TEST.

THE TESTS PERFORMED DURING EACH PHASE ARE LISTED BELOW.

PRE-FUEL TEST - TEST PERFORMED BEFORE FUEL IS INTRODUCED OR DELIVERED INTO THE UST SYSTEM.

THE FOLLOWING TESTS ARE RECOMMENDED TO BE PERFORMED BEFORE FUEL IS INTRODUCED INTO THE UST SYSTEM:

PRE-FUEL TEST	ESTIMATED TEST DURATION
STP SUMPS	MINIMUM 4 HOURS
DISPENSER SUMPS/CONTAINMENT	MINIMUM 4 HOURS
SPILL CONTAINMENT BUCKETS	MINIMUM 1 HOUR

TANKNOLOGY MAY USE ONE OF THE FOLLOWING TEST METHODS: HYDROSTATIC, PRESSURE OR VACUUM.

WATER USED TO PERFORM THESE PRE-FUEL TESTS MAY BE DISCHARGED ON SITE AS LONG AS THE WATER WAS NOT IN CONTACT WITH ANY FUEL.

CONTRACTOR PREPARATION FOR PRE-FUEL (BEFORE DELIVERY) TESTING

- THE TANKS, PIPING AND DISPENSERS MUST BE ACCESSIBLE TO INCLUDE ASPHALT AND CONCRETE THAT CAN BE DRIVEN UPON.
- 110 VOLT AC CURRENT MUST BE AVAILABLE
- ALL PRODUCT PIPING, TANK SUMP, SPILL BUCKET AND DISPENSER SUMP/LINER WORK MUST BE COMPLETE AND COMPONENTS ACCESSIBLE.
- TEST VALVES MUST BE INSTALLED FOR THE SECONDARY PRODUCT PIPING.
- DISPENSERS DO NOT NEED TO BE IN PLACE AND PRODUCT PIPING DOES NOT NEED TO BE PURGED.
- AS AN OPTION TO SIGNIFICANTLY REDUCE TIME ON SITE, THE CONTRACTOR SHOULD FLOOD ALL TANK SUMPS AND DISPENSER SUMPS WITH CLEAN WATER THE NIGHT BEFORE THE SCHEDULED TESTING. WATER MUST BE ADDED TO A MINIMUM OF 3 INCHES ABOVE THE HIGHEST PENETRATION OR SEAM IN THE SUMP OR PAN.
- THE CONTRACTOR SHOULD ALLOW SEVERAL HOURS TO ALLOW THE WATER LEVEL TO STABILIZE AND MARK THE LEVEL OF THE WATER WITH A PERMANENT MARKER.

POST FUEL TEST - TEST PERFORMED AFTER FUEL IS INTRODUCED OR DELIVERED INTO THE UST SYSTEM.

POST-FUEL TEST	ESTIMATED TEST DURATION
TANK INTERSTICE	MINIMUM 1 HOUR PER TANK
TANK PRIMARY (TIGHTNESS)	MINIMUM 2 1/2 HOURS
LINE/PIPE INTERSTICE	MINIMUM 1 HOUR PER LINE/PIPE
LINE PRIMARY (TIGHTNESS)	COMPLETED DURING TANK TEST
LINE LEAK DETECTOR	COMPLETED DURING TANK TEST
STAGE II (WHERE APPLICABLE)	MINIMUM 3 HOURS
*AUTOMATIC TANK GAUGE (ATG) CERTIFICATION	MINIMUM 2 HOURS
OTHER (AS REQUIRED BY STATE OR LOCAL ORDINANCE)	UNKNOWN

*DOES NOT INCLUDE NEW AUTOMATIC TANK GAUGE (ATG) INSTALLATION. CERTIFICATION OF NEW ATG IS THE RESPONSIBILITY OF THE INSTALLING CONTRACTOR.

NOTE: WATER USED TO PERFORM TESTING OF (STP) SUMPS, DISPENSER SUMPS AND/OR SPILL BUCKETS, AFTER FUEL IS INTRODUCED/DELIVERED INTO THE UST SYSTEM CANNOT BE DISCHARGED ON SITE.

WATER THAT IS OR WAS IN CONTACT WITH FUEL MUST BE PROPERLY DISCHARGED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND/OR LOCAL REGULATIONS.

CONTRACTOR PREPARATION FOR POST-FUEL (AFTER DELIVERY) TESTING

- THE FUEL LEVEL MUST BE BETWEEN 70-80 PERCENT OF TANK CAPACITY
- COMPARTMENTALIZED TANKS MUST HAVE FUEL LEVELS WITHIN 10" OF EACH OTHER.

ALL FUELING INCLUSIVE OF STAGE I AND STAGE II VAPOR RECOVERY AREAS THE FOLLOWING CONDITIONS MUST APPLY.

- ALL PRODUCT PIPING MUST BE THOROUGHLY PURGED OF AIR AND VAPORS.
- THE DISPENSERS MUST BE SET AND OPERATIONAL TO INCLUDE ALL VAPOR VACUUM PUMPS AND OTHER STAGE II EQUIPMENT FULLY FUNCTIONAL. REPLACE FILTER AFTER PURGING.
- ALL HANGING HARDWARE MUST BE INSTALLED AND OPERATIONAL
- THE TANKS, PIPING AND DISPENSERS MUST BE ACCESSIBLE TO INCLUDE ASPHALT AND CONCRETE THAT CAN BE DRIVEN UPON.
- 110 VOLT AC CURRENT MUST BE AVAILABLE
- TEST VALVES MUST BE INSTALLED FOR THE SECONDARY PRODUCT PIPING.

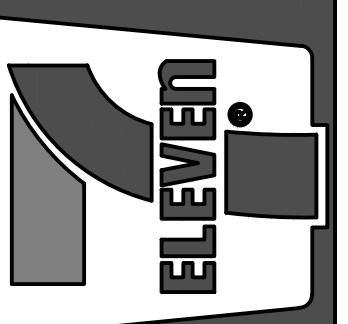
CONTRACTOR NOTIFICATION TO TESTING COMPANY (TANKNOLOGY):

IT IS RECOMMENDED THAT THE CONTRACTOR PROVIDE NOTIFICATION TO TANKNOLOGY OF THE NEED TO PERFORM TESTS AT LEAST 14 DAYS PRIOR TO THE ANTICIPATED TEST DATE. HOWEVER, TO ENSURE TESTS ARE CONDUCTED IN A TIMELY MANNER A MINIMUM OF FIVE (5) DAYS IS REQUIRED. TANKNOLOGY UNDERSTANDS THAT ACTUAL DATE MAY CHANGE, BY PROVIDING THEM WITH AS MUCH TIME AS POSSIBLE ALLOWING THEM TO MAINTAIN A TIME SLOT IN THEIR SCHEDULE TO SHIFT AS NECESSARY. THE CONTRACTOR MUST KEEP TANKNOLOGY ADVISED OF THE ANTICIPATED DATE AND TIME THAT THE TESTING WILL BE NEEDED AS PROJECTED DATES CHANGE.

Description	
Date	
Rev. #	

Proto 2022-04

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
FUELING SPECIFICATIONS



CORE STATES GROUP
CONCRETE
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 021079

46 East Main Street
Suite 200A
Suffolk, VA 23066
804.462.9700
core-states.com

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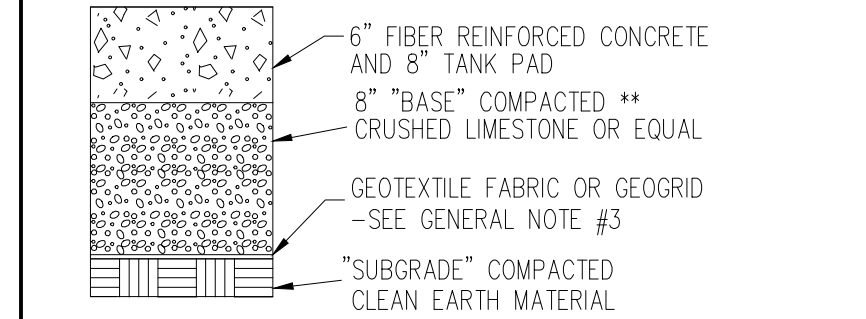


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CONCRETE PAVEMENT INSTALLATION

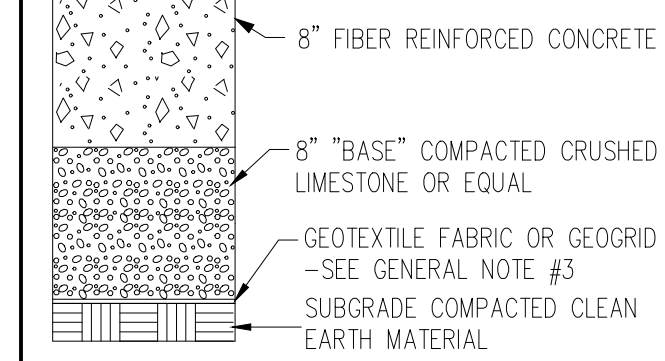
NORMAL STRENGTH PAVEMENT DETAIL

AUTO PARKING AREAS, AND OTHER LIMITED SERVICE TRAFFIC AREAS



HIGH STRENGTH PAVEMENT DETAIL

HEAVY LOAD TRAFFIC AREAS



CONCRETE PAVING GENERAL NOTES:

- SEE FUEL PROTOTYPE FOR ADDITIONAL REQUIREMENTS FOR TANK SLABS.
- SEE FUEL PROTOTYPE FOR ADDITIONAL REQUIREMENTS FOR DISPENSER ISLAND SLABS.
- A GEOTECHIL FABRIC OR GEOGRID IS ONLY NEEDED WHEN THE GEOTECHILICAL REPORT AND/OR A GEOTECH EXPERT REQUIRES IT FOR THE SPECIFIC SITE'S SOIL CONDITIONS.
- UNDISTURBED SOIL OR COMPACTED BACK FILL NOT LESS THAN 95% OF MODIFIED PROCTOR DENSITY (AASHTO-T-180).
- REFER TO CIVIL DESIGN GUIDELINES (CDG) FOR PAVEMENT LOCATION GUIDANCE.

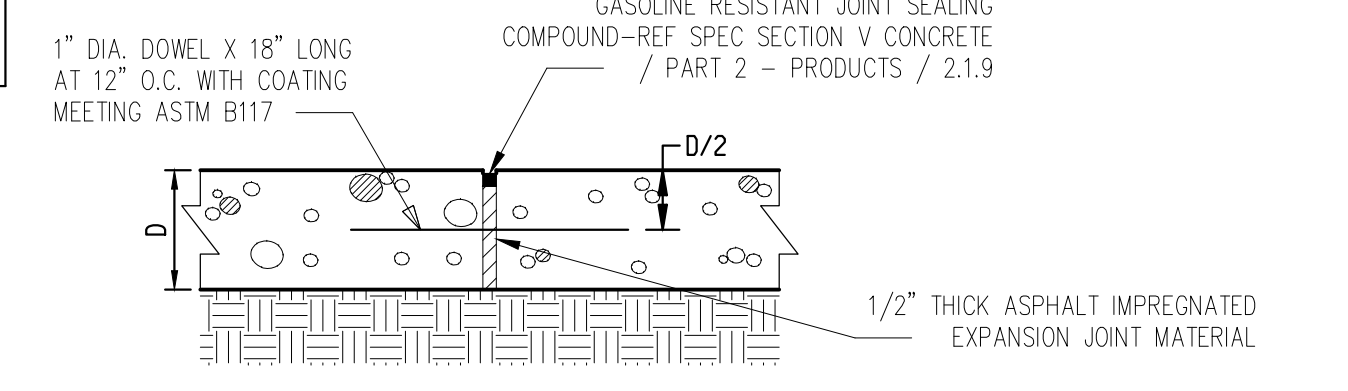
PAVING EXECUTION NOTES:

- LOCATE AND INSTALL CONSTRUCTION, ISOLATION, AND EXPANSION JOINTS AS INDICATED OR REQUIRED.
- PLACE CONCRETE IN A CONTINUOUS OPERATION WITHIN PLANNED JOINTS OR SECTIONS. DO NOT ADD WATER TO ADJUST SLUMP.
- FLOAT SURFACES TO TRUE PLANES WITHIN A TOLERANCE OF 1/4 INCH IN 10 FEET.
- TOOL EDGES AND JOINTS TO A RADIUS OF 1/4 INCH FOR SIDEWALKS.
- ALLOW CONCRETE PAVING TO CURE FOR A MINIMUM OF 28 DAYS AND DRY BEFORE STARTING PAVEMENT MARKING.
- APPLY TRAFFIC PAINT WITH MECHANICAL EQUIPMENT TO A MINIMUM WET FILM THICKNESS OF 15 MILS.
- PROTECT CONCRETE PAVING FROM DAMAGE. EXCLUDE TRAFFIC FROM PAVING FOR AT LEAST 14 DAYS.

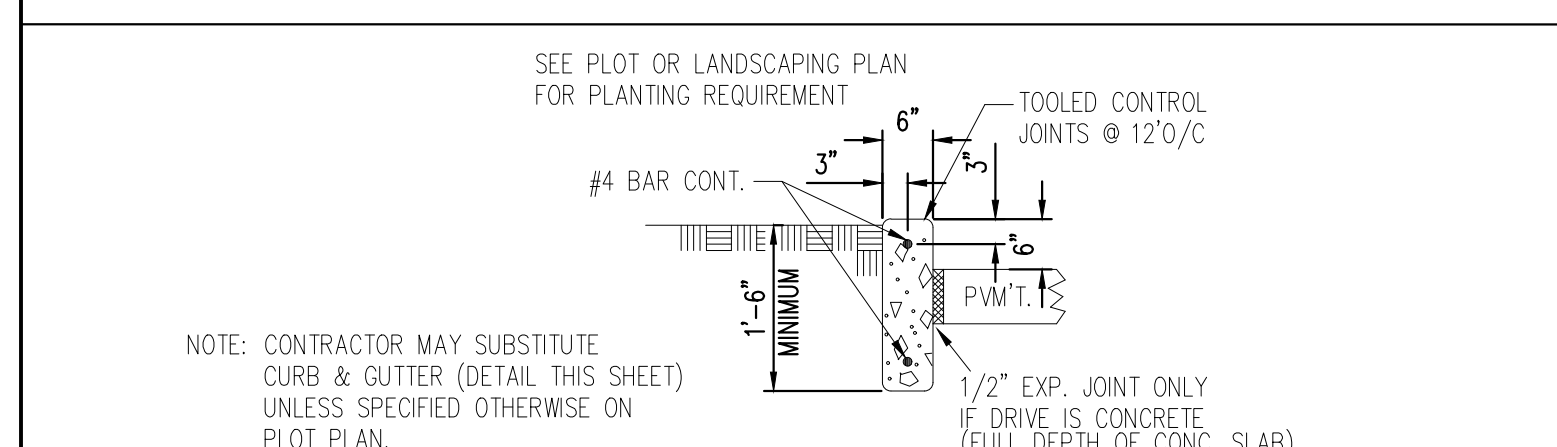
- * CONTRACTOR MUST REFER TO THE GEOTECH REPORT FOR PAVEMENT AND FOUNDATION DESIGN AND CONSTRUCTION RECOMMENDATIONS
- ** COMPACTION IS NOT REQUIRED OVER THE TANK PIT(S)

CONCRETE PAVEMENT JOINT DETAILS

TYPE OF JOINT	USE DESCRIPTION	DETAIL
A. CONTROL JOINT 1. SAWN 2. PREMOLD STRIP 3. HAND FORMED AND SAWN AT CRITICAL JOINTS	USE SPECIFICATION ACI 360R-06 TO PLACE CONTROL JOINTS SO AS TO UNIFORMLY DIVIDE SLABS INTO NOMINAL 12' x 12' (NORMAL STRENGTH) OR 16' x 16' (HIGH STRENGTH) SECTIONS. (SEAL CONTROL JOINTS ONLY IF SPECIFIED ON DRAWINGS OR BY OWNER'S REPRESENTATIVE.)	
B. EXPANSION JOINT (ISOLATION JOINT)	PLACE AGAINST CONCRETE TANK SLAB, ADJUTING ROADWAY PAVEMENTS AND STRUCTURES (BUILDINGS, CANOPY COLUMNS, CATCH BASINS, ETC.)	
C. CONSTRUCTION JOINT	CONSTRUCTION JOINTS MUST BE APPROVED BY OWNER'S REPRESENTATIVE. PLACE WHEN CONTINUOUS POUR OPERATIONS ARE SUSPENDED FOR MORE THAN 30 MINUTES.	

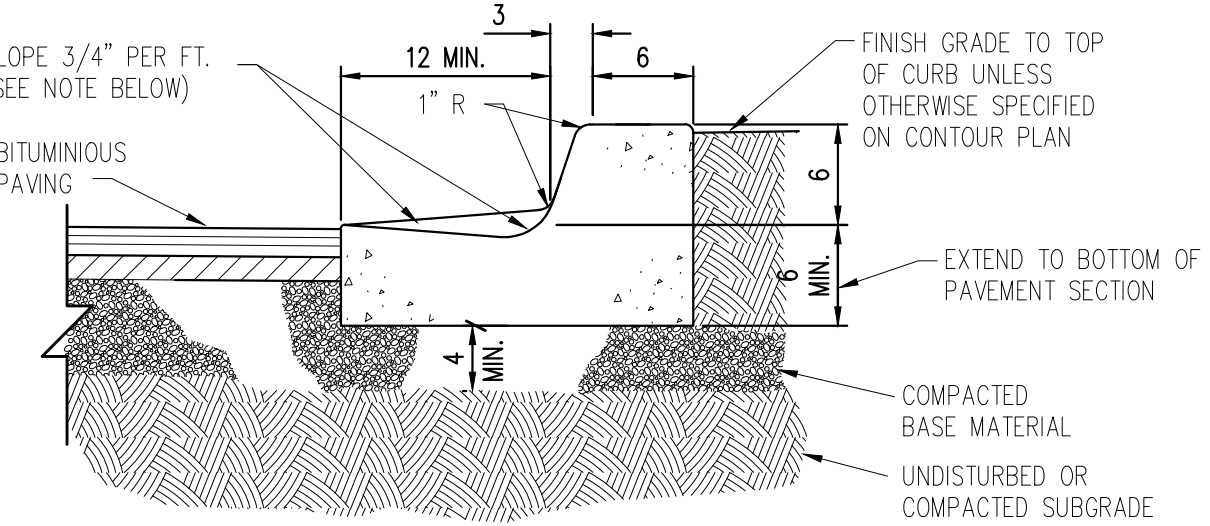


CONCRETE CURBS



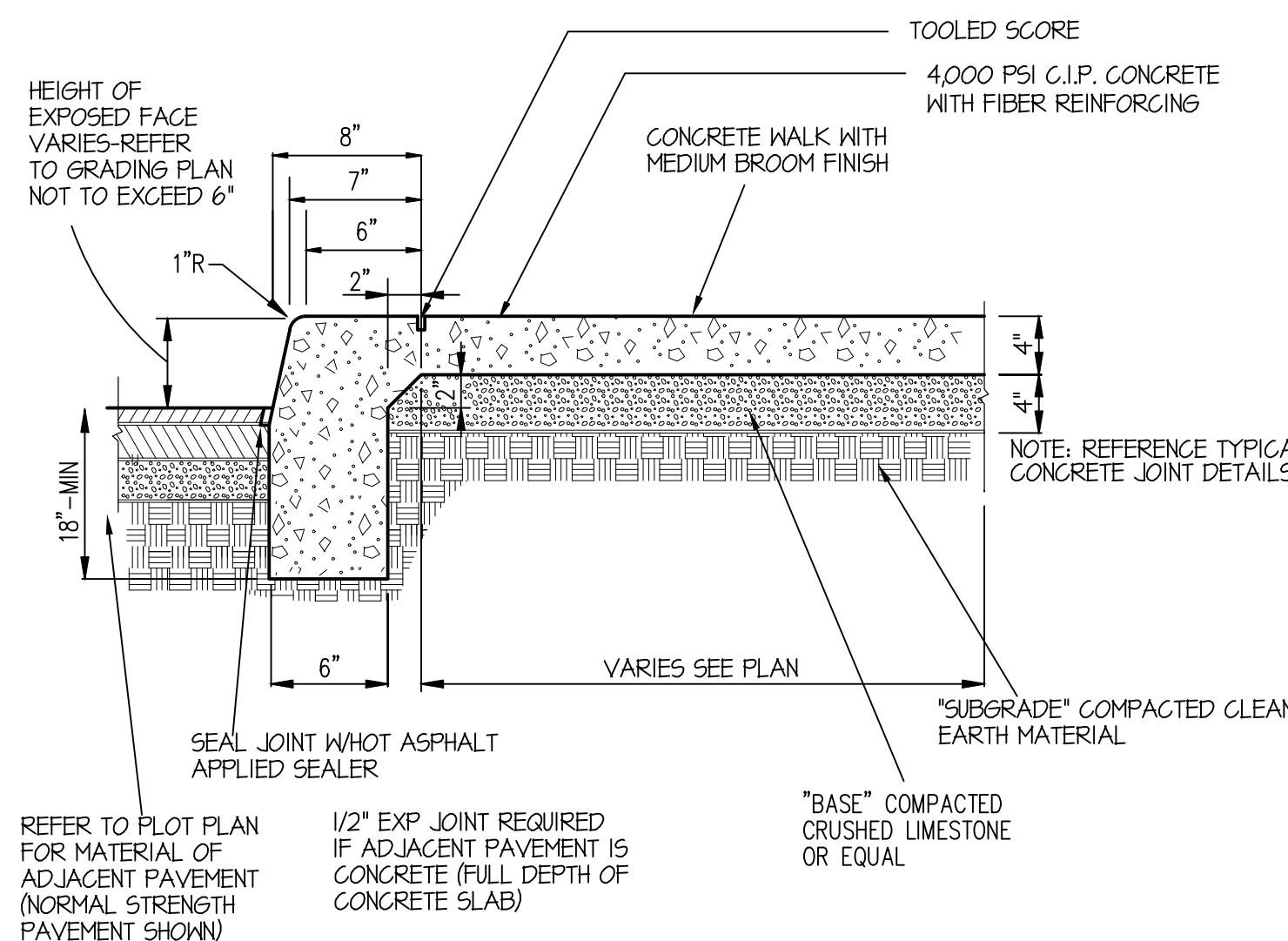
CAST-IN-PLACE CONCRETE CURB

PREFERRED



NOTE: GUTTER TO BE SLOPED DOWNGRADIENT TOWARDS CURB WHEN SITE GRADE IS TOWARDS CURB. GUTTER TO BE SLOPED DOWN GRADIENT AWAY FROM CURB WHEN SITE GRADE IS AWAY FROM CURB. MAY BE MACHINE FORMED.

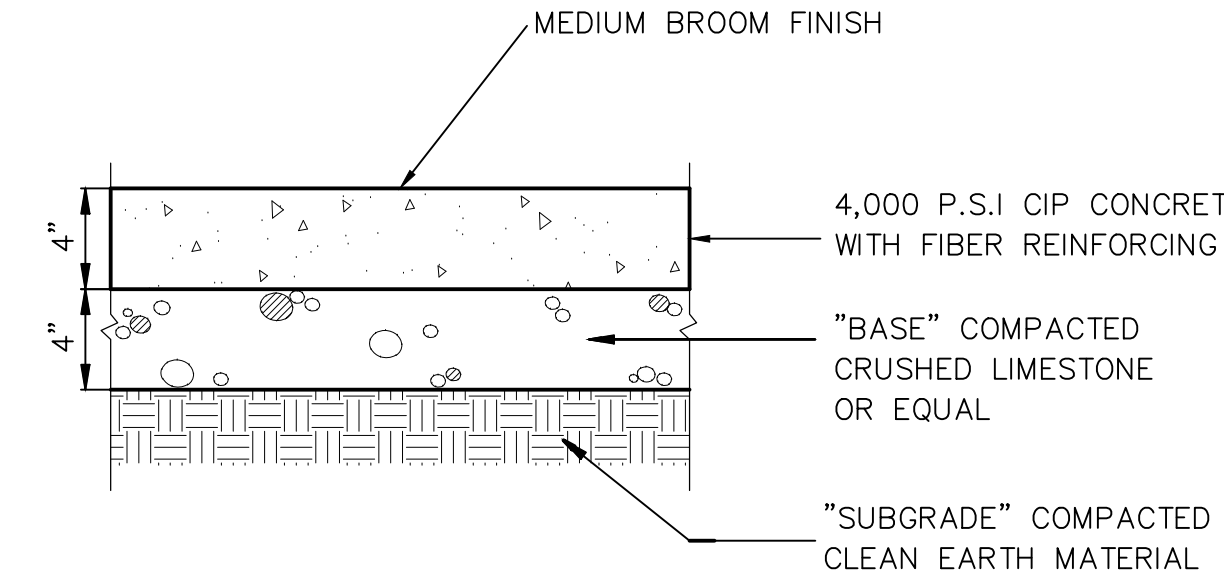
STRAIGHT CONCRETE CURB & GUTTER



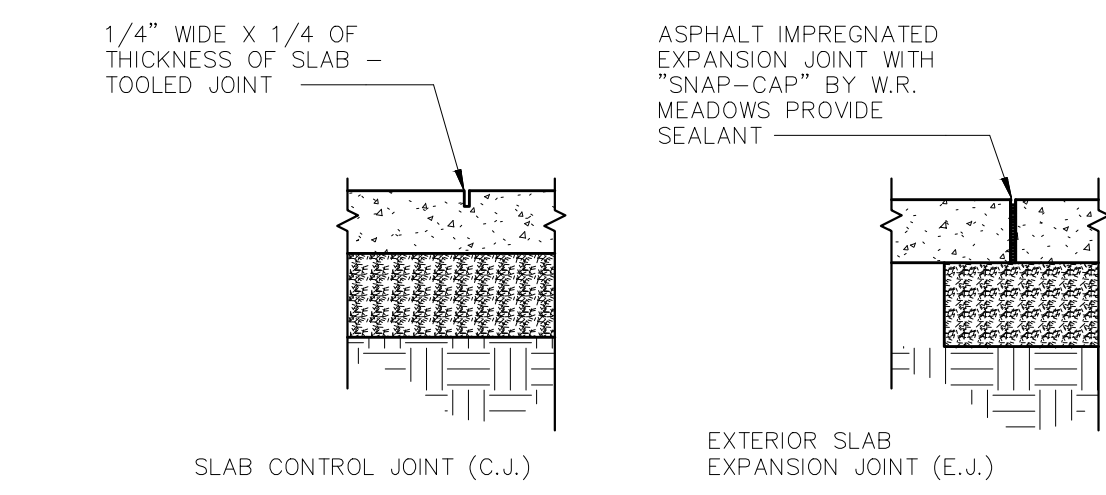
REFER TO PLOT PLAN FOR MATERIAL OF ADJACENT PAVEMENT (NORMAL STRENGTH PAVEMENT SHOWN)

1/2" EXP JOINT REQUIRED IF ADJACENT PAVEMENT IS CONCRETE (FULL DEPTH OF CONCRETE SLAB)

- NOTE:
- TYPICALLY THERE IS 0" CURBING AROUND THE BUILDING
 - EXCEPTION: CA REQUIRES SIDEWALK CURBING AROUND THE BUILDING



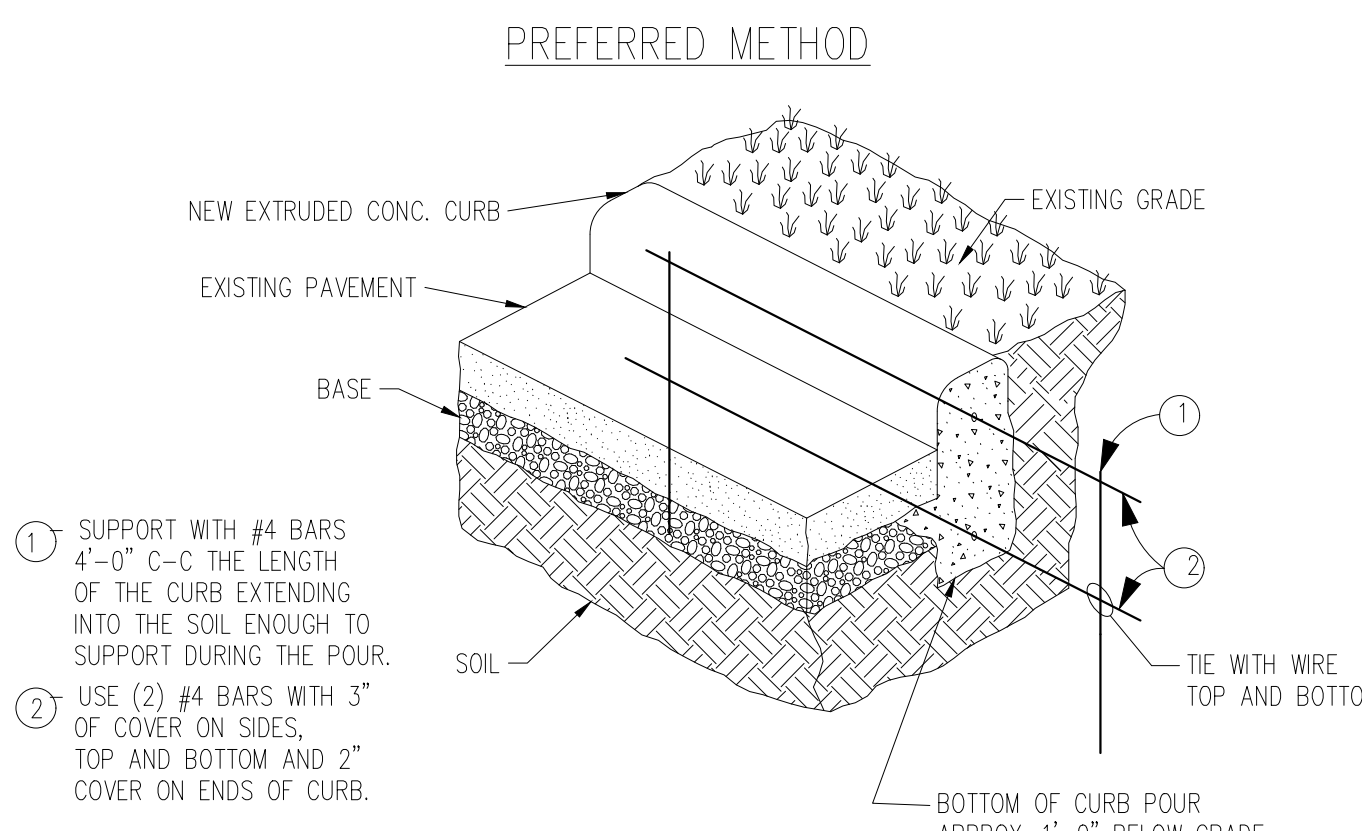
CONCRETE SIDEWALK PAVEMENT



FOR 5' OR 8' WALKS, INSTALL A CONTROL JOINT EVERY 5' IN 10' WALKS. INSTALL A CONTROL JOINT EVERY 5' AND ONE IN THE CENTER (5' FROM THE BACK OF THE BUILDING FOR EXAMPLE)

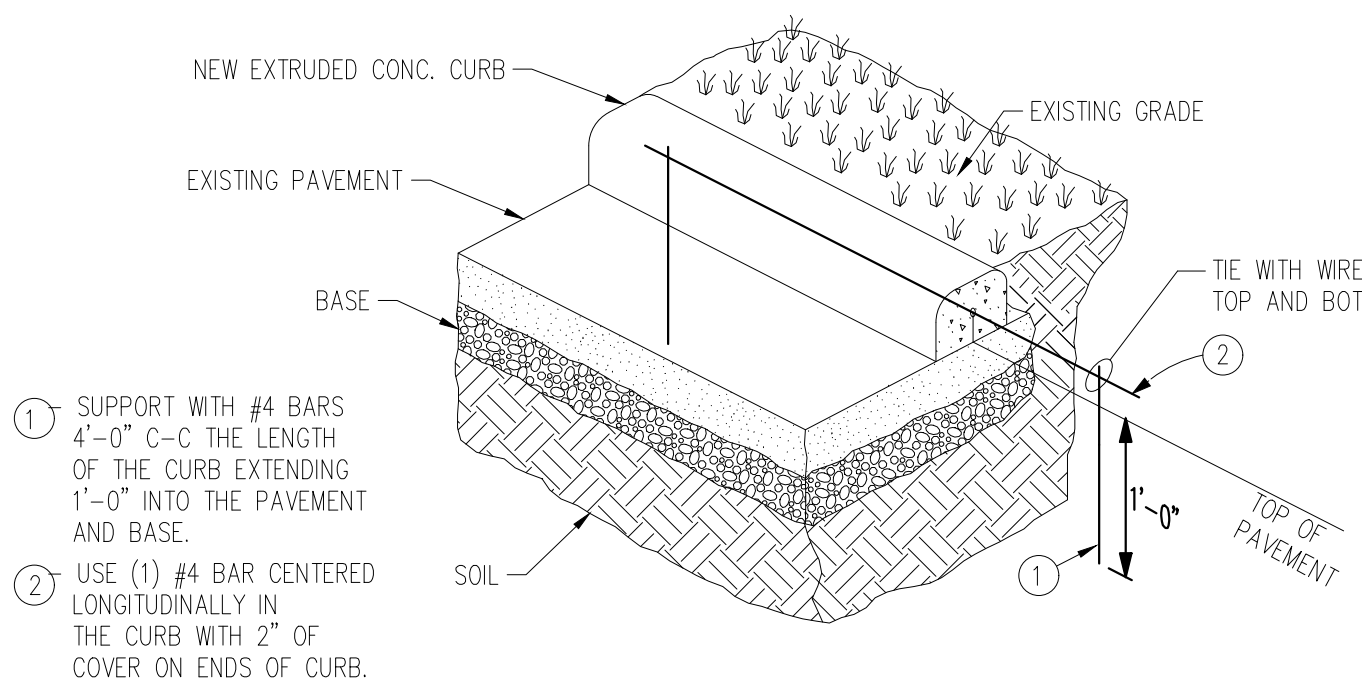
DETAILS ONLY APPLY TO BUILDING SIDEWALK. THEY DO NOT APPLY TO INTERIOR SLAB OR SITE CONCRETE.

SIDEWALK JOINTS



- SUPPORT WITH #4 BARS 4'-0" C-C THE LENGTH OF THE CURB EXTENDING INTO THE SOIL ENOUGH TO SUPPORT DURING THE POUR.
- USE (2) #4 BARS WITH 3" OF COVER ON SIDES, TOP AND BOTTOM AND 2" COVER ON ENDS OF CURB.

ALTERNATE METHOD



- SUPPORT WITH #4 BARS 4'-0" C-C THE LENGTH OF THE CURB EXTENDING 1'-0" INTO THE PAVEMENT AND BASE.
- USE (1) #4 BAR CENTERED LONGITUDINALLY IN THE CURB WITH 2" OF COVER ON ENDS OF CURB.

COLD WEATHER CURB INSTALLATION

FOR USE WHEN COLD WEATHER CONDITIONS CAUSE ASPHALT PAVEMENT TO BE PLACED BEFORE THE CONCRETE CURB.

ASPHALT PAVEMENT INSTALLATION

ASPHALT SEALING

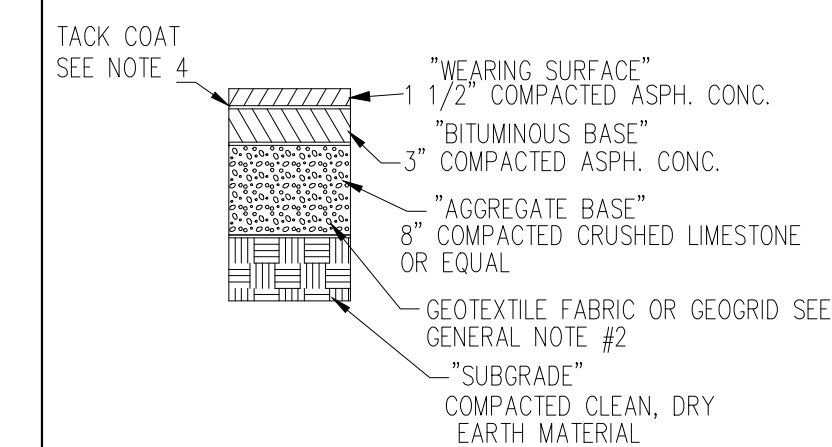
- SURFACE CLEANING:**
THE SURFACE TO BE SEALED SHALL BE FREE FROM DIRT AND OTHER FOREIGN MATTER. ANY ACCUMULATIONS OF OIL OR GREASE SHALL BE CLEANED OFF THE PAVEMENT WITH DETERGENT SOLUTION, THE RESIDUE OF WHICH SHALL BE THOROUGHLY WASHED AWAY WITH CLEAN WATER BEFORE SEALANT IS APPLIED.
- SEALING:**
THE SEAL COATING MATERIAL SHALL BE COMPRISED OF A RUBBERIZED COAL-TAR PITCH EMULSION AND SHALL MEET OR EXCEED FEDERAL SPECIFICATION R-P-355e. SUPPLIED IN CONCENTRATED FORM, IT SHALL BE DILUTED A MINIMUM OF 15% AND A MAXIMUM OF 25% WITH FRESH, CLEAN WATER. THE SEALANT MATERIAL SHALL CONTAIN 5-6 LBS. PER GALLON OF FINE, CLEAN, DRY SILICA SAND MEETING THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
#16	100
#30	80-100
#50	10-60
#100	0-5

THE SEALANT SHALL BE APPLIED TO THE PAVEMENT IN TWO COATS AT THE RATE OF 0.08 TO 0.12 GALLONS PER SQUARE YARD. A LATEX ADDITIVE MAY ALSO BE ADDED TO THE SEALANT TO ALLOW FOR QUICKER DRYING TIME IN THOSE AREAS SPECIFIED BY OWNER'S REPRESENTATIVE.

NORMAL STRENGTH PAVEMENT

PARKING AREAS, AND OTHER LIMITED SERVICE TRAFFIC AREAS
GRANULAR BASE (PREFERRED)

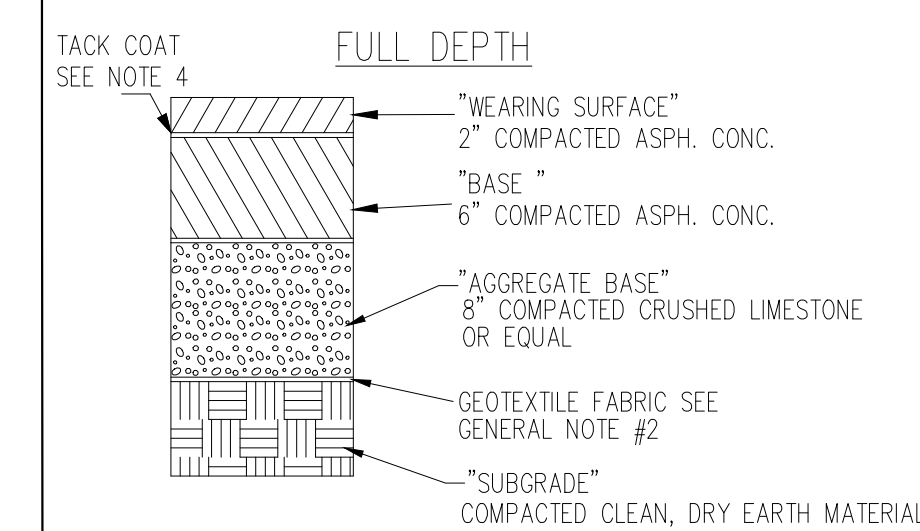


ASPHALT PAVEMENT GENERAL NOTES:

- ALL WORK SHALL BE IN CONFORMANCE WITH OWNER'S SPECIFICATIONS FOR ASPHALTIC CONCRETE PAVING.
- A GEOTECHIL FABRIC OR GEOGRID IS ONLY NEEDED WHEN THE GEOTECHILICAL REPORT AND/OR A GEOTECH EXPERT REQUIRES IT FOR THE SPECIFIC SITE'S SOIL CONDITIONS.
- FOR SUBSURFACE DRAINAGE OR STABILITY ISSUES DURING CONSTRUCTION (UNDER DIRECTION BY GEOTECH EXPERT):
 - SUBSURFACE DRAINAGE ISSUE - REFER TO GUIDELINES FOR DRAINAGE ISSUES TO DERIVE ACCEPTABLE OPTIONS.
 - SUBSURFACE STABILITY ISSUE - INSTALL GEOGRID PER GEOTECH EXPERT RECOMMENDATION.
- REFER TO CIVIL DESIGN GUIDELINES (CDG) FOR PAVEMENT LOCATION GUIDANCE.

HIGH STRENGTH PAVEMENT

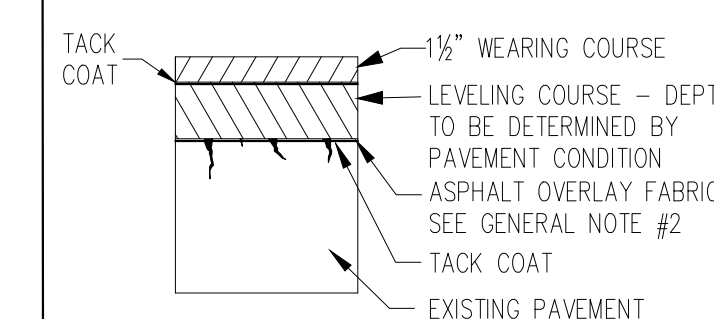
HEAVY LOAD TRAFFIC AREAS



* CONTRACTOR MUST REFER TO THE GEOTECH REPORT FOR PAVEMENT AND FOUNDATION DESIGN AND CONSTRUCTION RECOMMENDATIONS

ASPHALT OVERLAY DETAIL

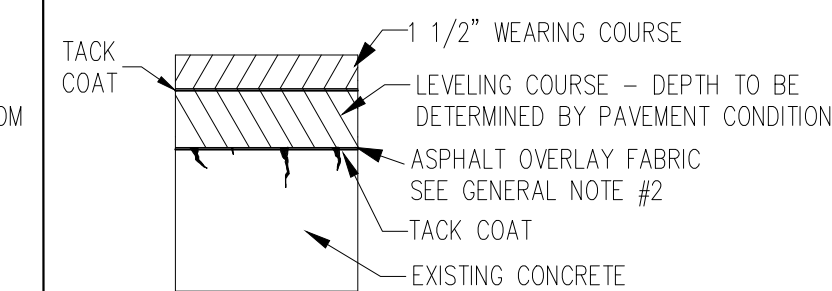
ASPHALT SUBBASE



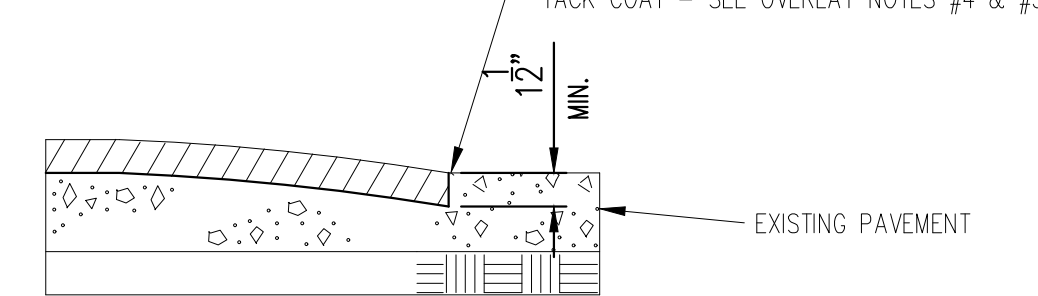
OVERLAY DETAIL NOTES:

- THE OVERLAY SHALL BE PLACED IN ACCORDANCE WITH OWNER'S SPECIFICATIONS.
- ALL FAILED AREAS SHALL BE REPAIRED WITH PROPER PATCHES BEFORE OVERLAY IS PLACED.
- IF THE SURFACE IS DISTORTED, THE CONTRACTOR SHALL CONSTRUCT LEVELING COURSES TO RESTORE PROPER LINE AND CROSS SECTION.
- THE PAVEMENT SHALL BE THOROUGHLY CLEANED AND A TACK COAT OF ASPHALT SHALL BE APPLIED BEFORE THE OVERLAY IS PLACED.
- ALL VERTICAL SURFACES COMING IN CONTACT WITH THE OVERLAY SHALL BE SPRAYED OR PAINTED WITH A UNIFORM COATING OF EMULSIFIED ASPHALT IMMEDIATELY PRIOR TO PAVEMENT CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR ENSURING PROPER SURFACE DRAINAGE, PONDING OR PUDDLING OF WATER ON THE FINAL SURFACE WILL BE UNACCEPTABLE.
- REPAIR IN KIND ANY EXISTING TRAFFIC LOOPS AND/OR RELATED SENSOR EQUIPMENT THAT IS REMOVED OR DAMAGED DUE TO PAVEMENT MILLING ACTIVITY

CONCRETE SUBBASE



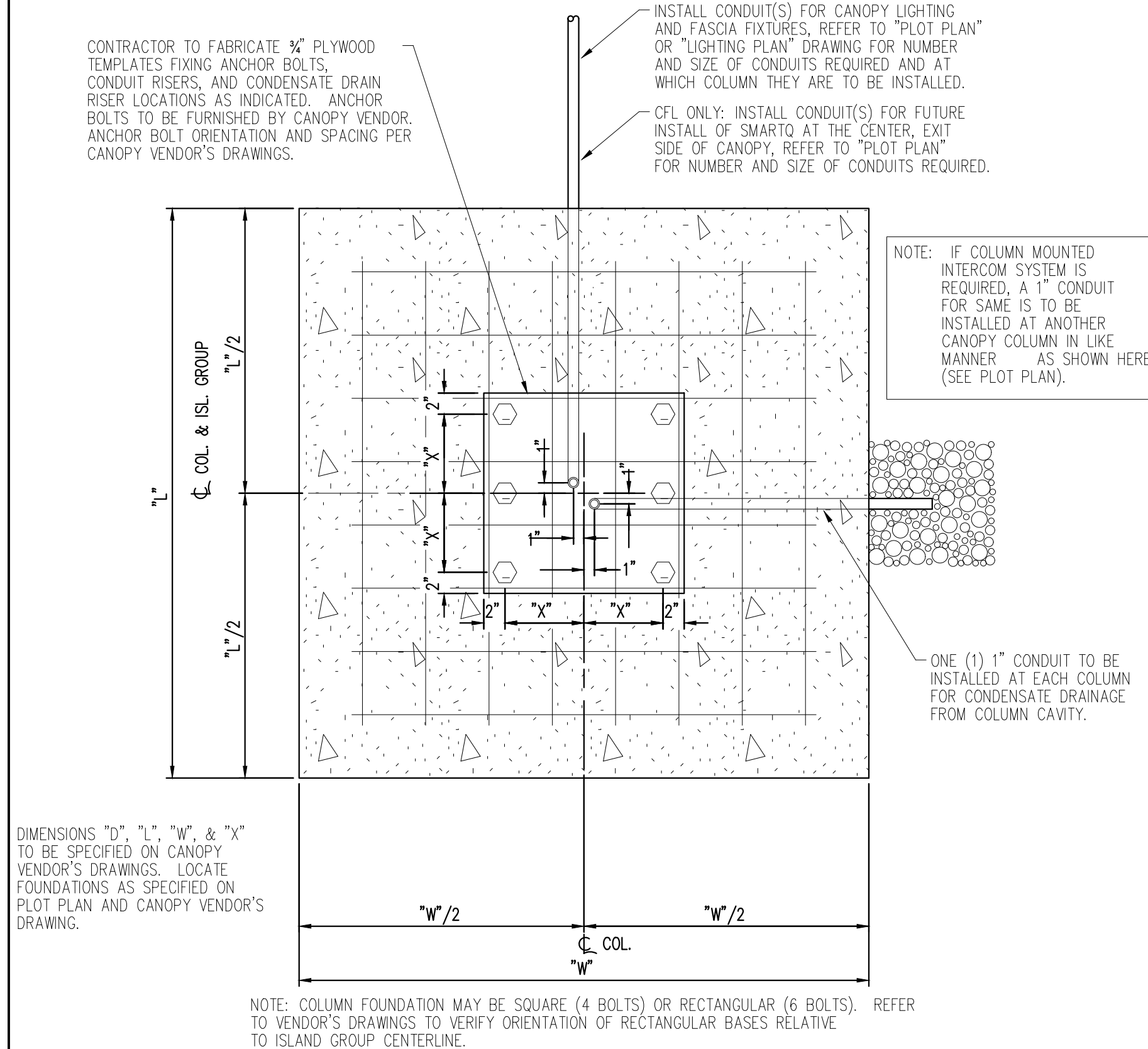
EDGE DETAIL



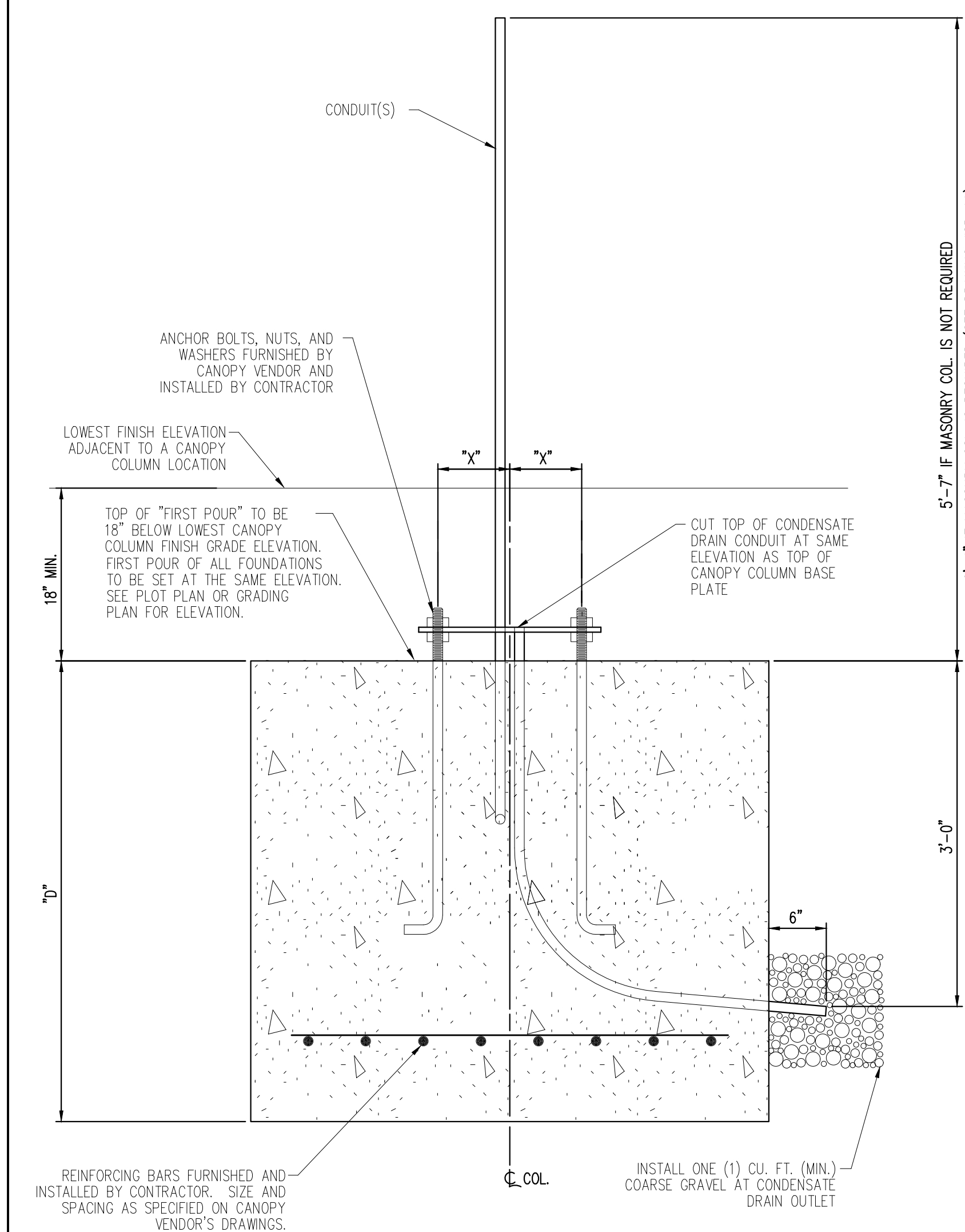
WHERE OVERLAY IS NOT SPECIFIED FOR ENTIRE EXISTING SURFACE, THE INTERFACE BETWEEN THE NEW OVERLAY AND THE OLD PAVEMENT SURFACE SHALL BE PER THE ABOVE DETAIL. FEATHERING OF THE ASPHALT TO MEET THE EXISTING PAVEMENT WILL NOT BE ACCEPTABLE.

Job#: SPW-36-100-SEI	Scale: AS NOTED	Date: 02/14/23	Drawn By: RMC	Checked By: CGC
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<p>7-ELEVEN, INC. 3200 HACKBERRY ROAD, IRVING TEXAS 75063 7-ELEVEN #46895 - SPEEDWAY #7645 1542 ROUTE 9 WAPPINGERS FALLS, NY 12590 FUELING REMODEL PAVEMENT AND CURBING DETAILS</p>				
<p>CORE STATES GROUP CONCRETE, ARCHITECTURE AND ENGINEERING P.C. CERTIFICATE OF AUTHORIZATION NO. 000079 46 East Main Street Suite 200A, NJ 08876 908.462.9700 core-states.com</p>				
<p>STATE OF NEW YORK JULY 19 2023 092855 PROFESSIONAL ENGINEER 09-19-2023</p>				
<p>SHEET: CPC-1 FUELING - USA</p>				

FIRST POUR

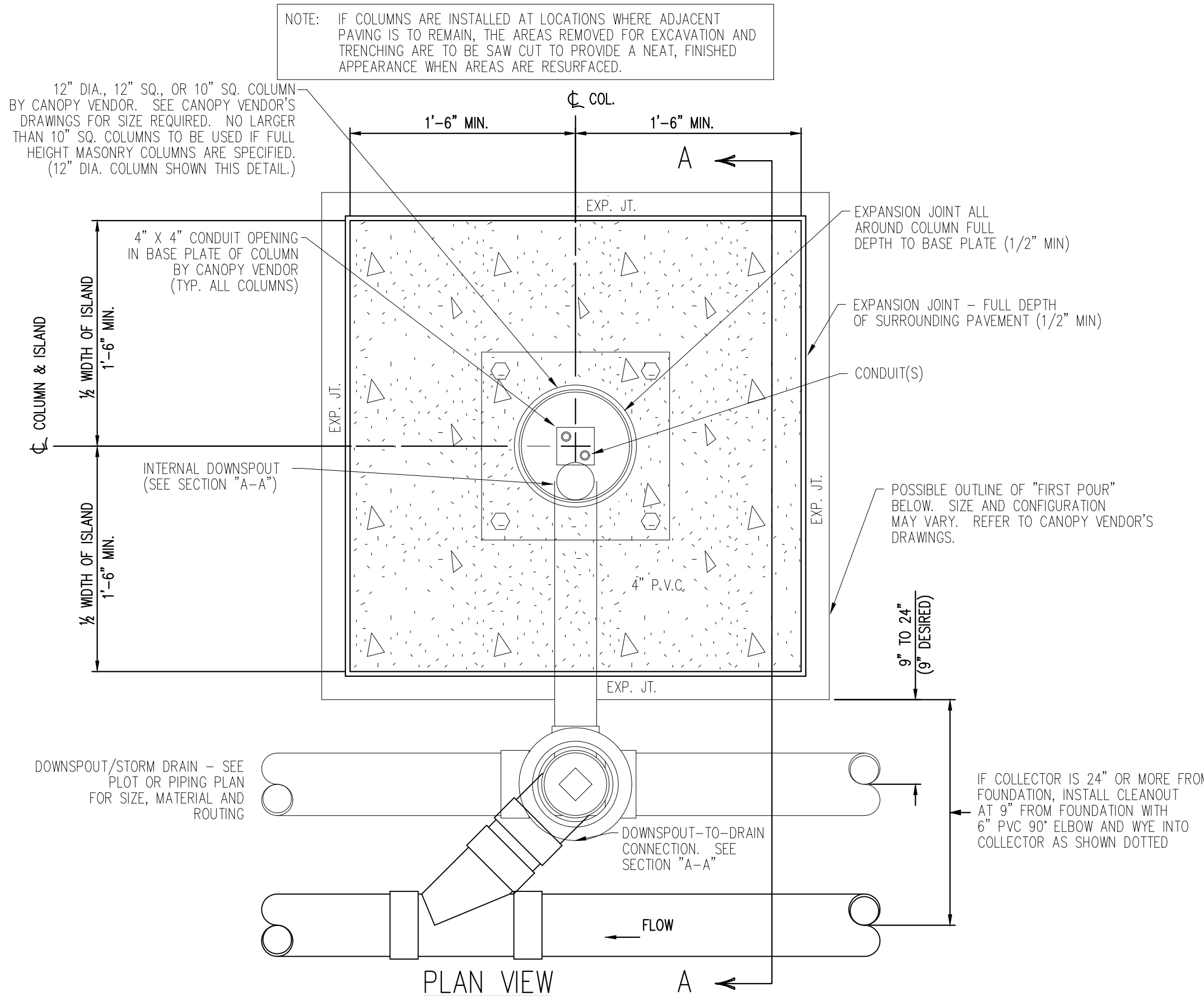


PLAN VIEW

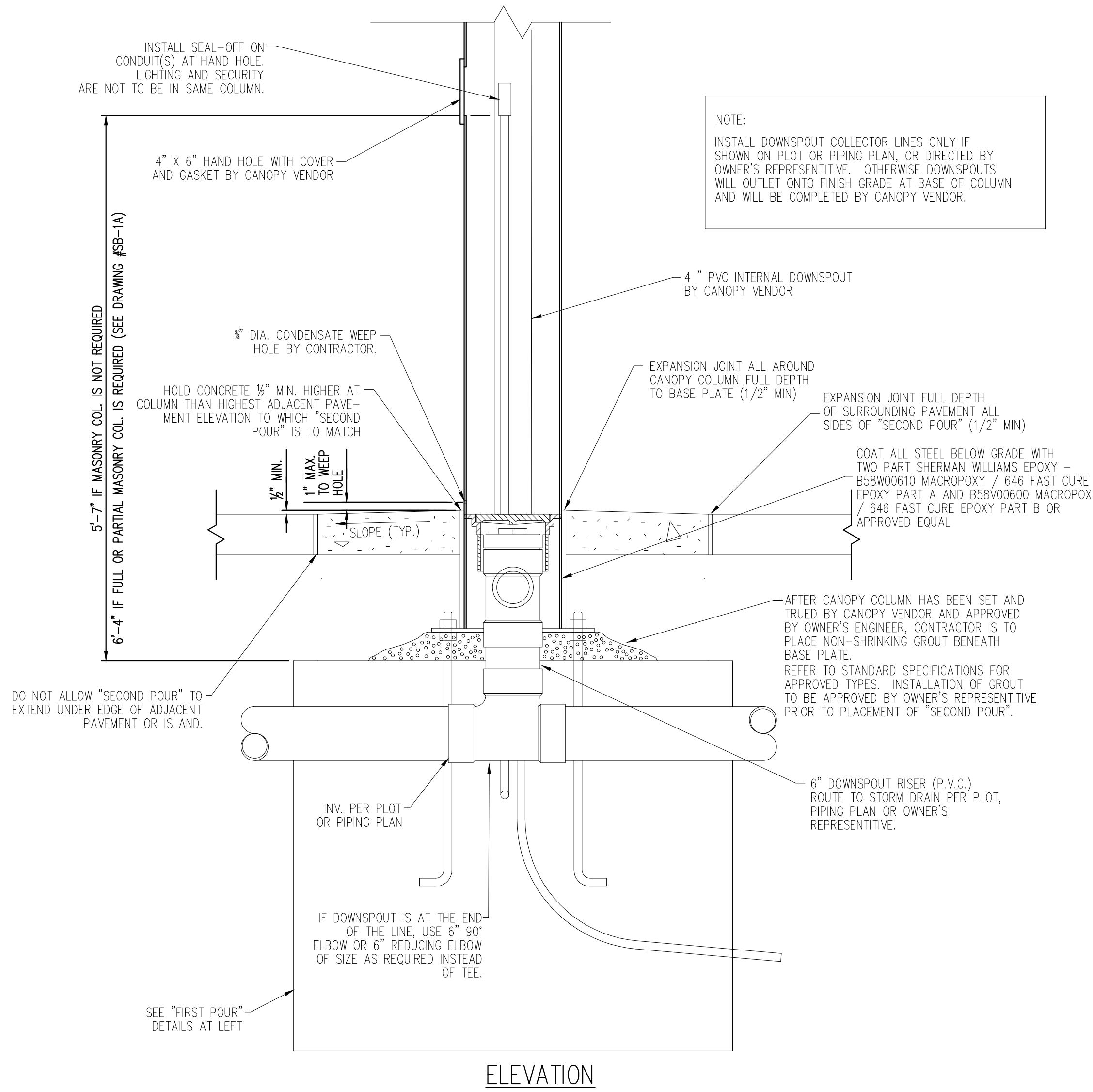


ELEVATION

SECOND POUR

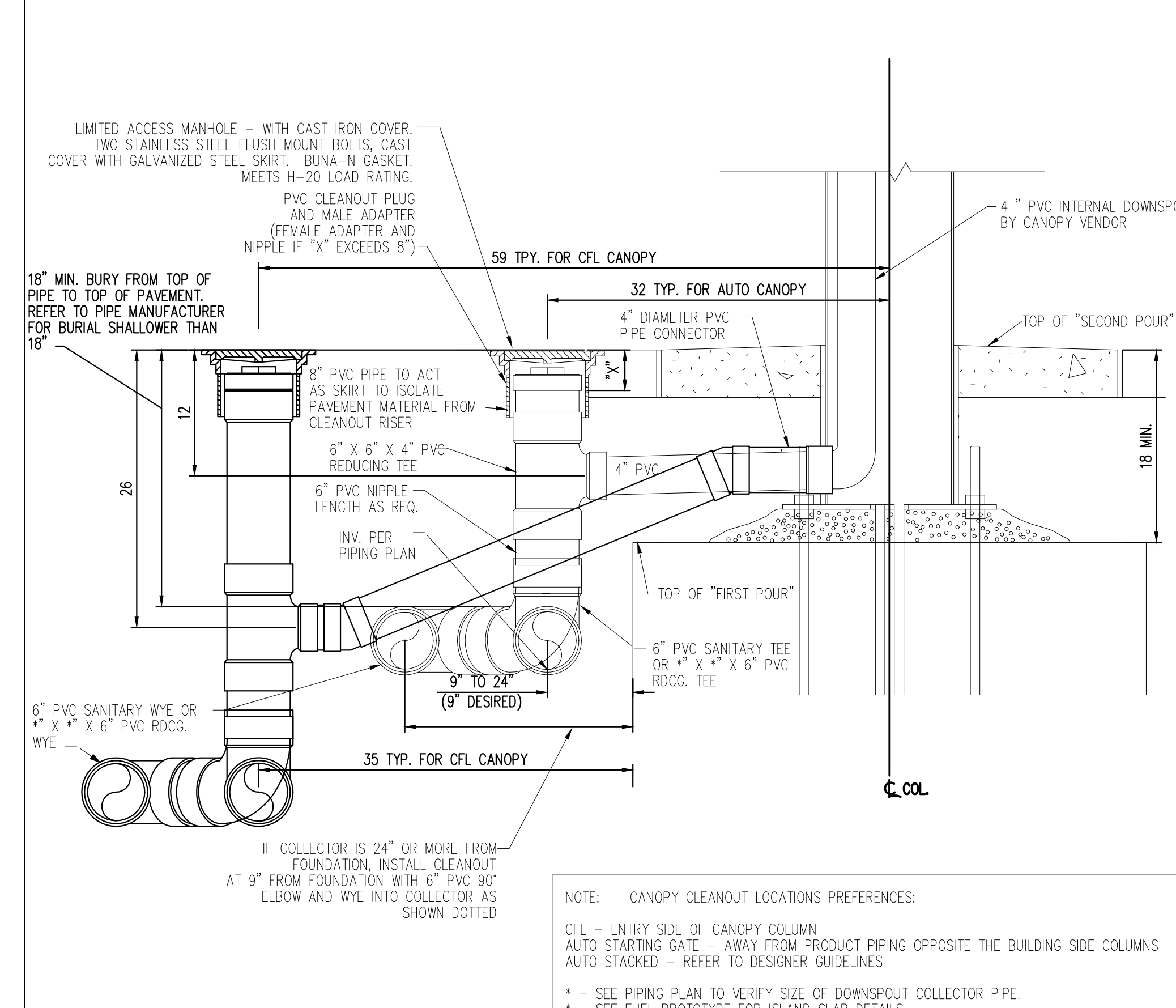


PLAN VIEW



ELEVATION

CANOPY DRAIN DETAILS



SECTION "A-A"
INTERNAL DOWNSPOUT

Rev. #	Date	Description
Proto 2022-04		

7-ELEVEN, INC.
3200 HACKBERRY ROAD, IRVING TEXAS 75063
7-ELEVEN #46895 - SPEEDWAY #7645
1542 ROUTE 9
WAPPINGERS FALLS, NY 12590
FUELING REMODEL
STANDARD CANOPY BASE DETAILS

CORE STATES GROUP
CONCRETE
ARCHITECTURE AND ENGINEERING P.C.
CERTIFICATE OF AUTHORIZATION
NO. 020379

46 East Main Street
Suite 200A, NJ 08876
908.462.9700
core-states.com

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